

STREET AND STREETScape STANDARDS

24M.208.010 Purpose and Applicability

A. PURPOSE.

This Section identifies the street and streetscape types allowed within the Midtown Area, and provides design standards for each type, to ensure that proposed development is consistent with the City's goals for character and quality of the public realm of the street.

B. APPLICABILITY.

1. This Section applies to alleys and the various configurations of Main Street and Thompson Boulevard respectively. The proposed standards herein are conceptual and will be studied and may be revised accordingly during the Mobility Plan and Midtown Community Plan. Development projects do not need to comply with Section 24M.208.
2. Corner lots located on Main Street and Thompson Boulevard shall provide a 10 foot wide sidewalk along the side street.

2005 GENERAL PLAN

Main Street

Currently a commerce oriented area with a limited amount of mixed use development, this corridor displays the broadest range of architectural types and styles in the city, as well as the widest spectrum of transect characteristics. It has the most potential for increased mixed use and housing with improved streetscape and pedestrian enhancement to slow traffic.

Thompson Boulevard

A commercial thoroughfare in need of streetscape improvements and pedestrian amenities, this corridor is much like Main Street in that it boasts tremendous history as a "gateway to Ventura" and epitomizes a beach town character. It is a natural for a major transit or streetcar corridor, where nodes of mixed-use development and pedestrian and bike enhancement could support parallel neighborhoods and increase access to the ocean.

Chapter 3: Our Well Planned and Designed Community

Existing Condition



The conceptual photo-simulation shows one possible scenario of incremental improvements at the Thompson Boulevard and Santa Rosa Street intersection possible under the Midtown Corridors Development Code.

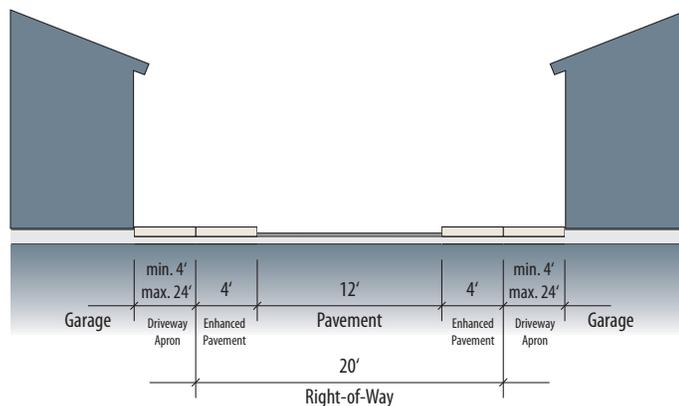
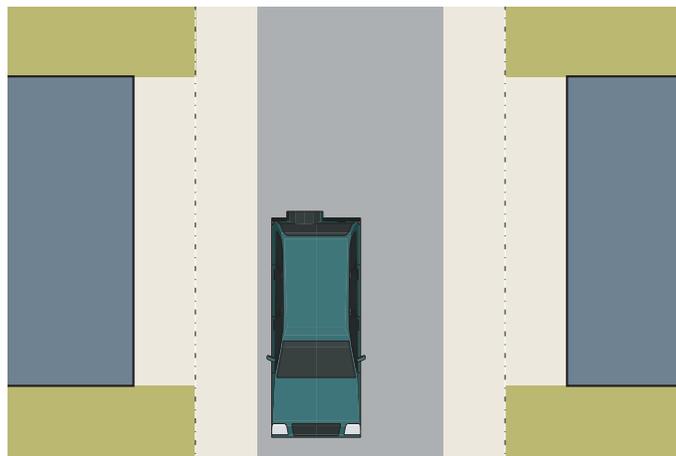
24M.208.020 Alley - Proposed

A. DESCRIPTION.

Alleys are located in the rear of lots and provide the primary vehicular access to residential, as well as non-residential property. Garages are oriented to and accessed from alleys. Garages are located a minimum of 4' from the rear property line and may be set back further to allow for parking in front of the garage (maximum setback: 24'). All pavement beyond the 12' travel lane shall be enhanced pavement (E.P.). Enhanced pavement can be permeable or non-permeable, and includes types such as brick, stone or grass pavers, exposed aggregate concrete, or stamped and stained concrete. Trash cans, gas and electric meters and other utilities are located in the alleys.

Thoroughfare Type
Transect Zone Assignment
Right-of-Way Width
Pavement Width
Movement
Design Speed
Pedestrian Crossing Time
Traffic Lanes
Parking Lanes
Curb Radius
Public Frontage Type
Walkway Type
Planter Type
Curb Type
Landscape Type
Transportation Provision

Alley	
Rear Lane	
T4.5, T5.2	
20' max.	
12'	
Yield Movement	
10 MPH	
3.5 seconds	
1 lane	
None	
25'	
None	
None	
Between driveways	
Inverted Crown or Rolled	
Trees at 30' o.c. Avg.	
None	



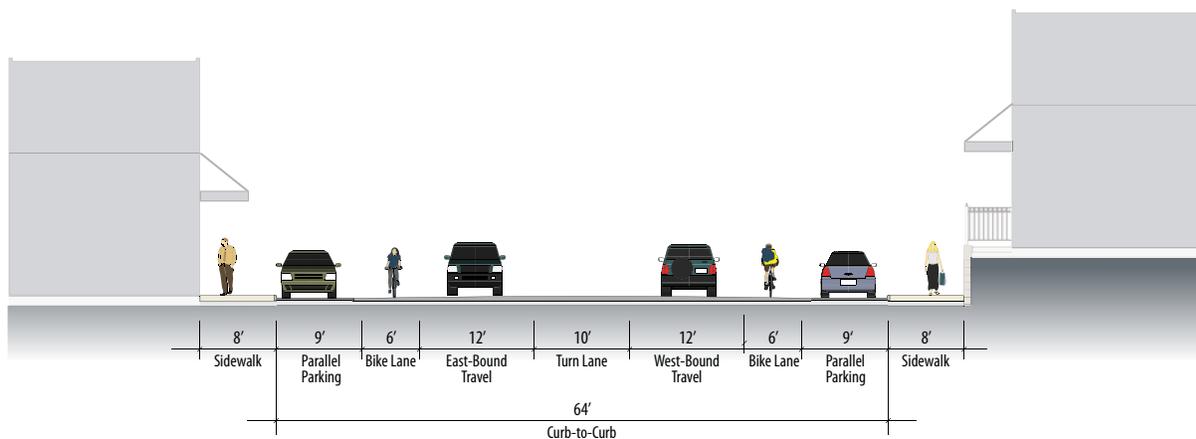
24M.208.030 Main Street - A

A. EXISTING CONDITION:

From approximately Lincoln Drive to N. Catalina St., Main Street is defined by one and two-story buildings which are primarily of a residential character. Many of these buildings were constructed in the early 1920's and reflect a distinct and historically significant architectural period. This segment of Main Street represents the edge of the surrounding neighborhoods and has therefore been identified as a T4.5 Zone. The northern streetscape is defined by stoops and dooryards towards the west and shopfronts toward the east. The southern streetscape is defined almost entirely by shopfronts. It is an urban thoroughfare with two travel lanes, a turning lane, a bike lane and parallel parking each side. The pedestrian experience is fairly austere as no landscape elements exist, only archaic serpent -head street lights and power poles.

Thoroughfare Type	
Transect Zone Assignment	
Right-of-Way Width	
Pavement Width	
Movement	
Design Speed	
Pedestrian Crossing Time	
Traffic Lanes	
Parking Lanes	
Curb Radius	
Public Frontage Type	
Walkway Type	
Planter Type	
Curb Type	
Landscape Type	
Transportation Provision	

Existing Main Street A	
Street	
T4.5	
80'	
64'	
Free Movement	
35 MPH	
8 - 11 seconds	
3 lanes	
Both sides @ 9'	
10'	
--	
Sidewalk, 8' min.	
None	
Curb	
None	
None	



B. PROPOSED APPROACH:

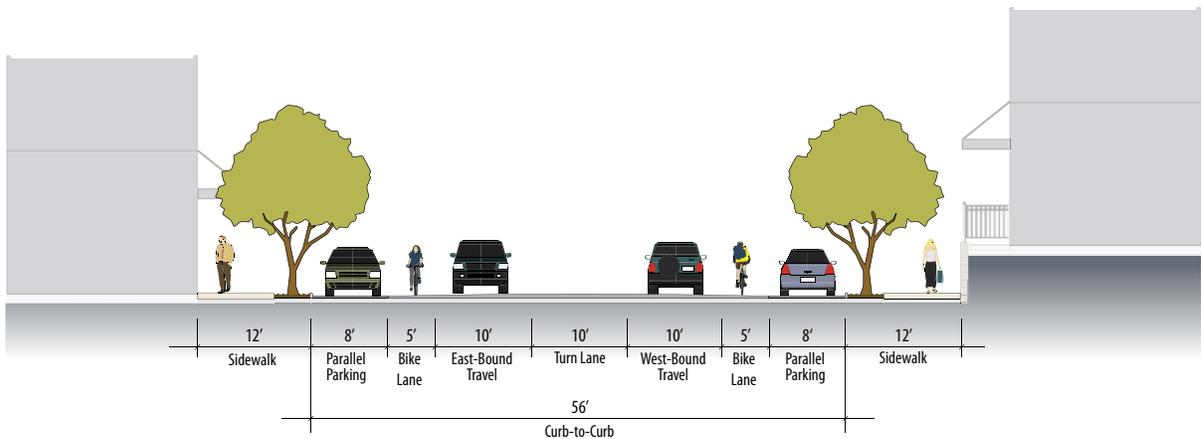
The proposed alterations are focused on maintaining the existing street’s residential character and increasing the quality of the pedestrian experience. Main Street is almost exclusively a commercial street with a few mixed-use buildings. Future development on Main Street should maintain the established streetscape and land use pattern. Buildings should not exceed 2 stories in height and, where appropriate, be provided with awnings that strengthen a sense of enclosure for shoppers and strollers. The pedestrian experience should be enhanced with sidewalks that have street trees in wells and pedestrian-scale lighting. The existing on-street parallel parking helps to support the businesses along both sides of Main Street.

Thoroughfare Type	
Transect Zone Assignment	
Right-of-Way Width	
Pavement Width	
Movement	
Design Speed	
Pedestrian Crossing Time	
Traffic Lanes	
Parking Lanes	
Curb Radius	
Public Frontage Type	
Walkway Type	
Planter Type	
Curb Type	
Landscape Type	
Transportation Provision	

Proposed Main Street A	
Street	
T4.5	
80'	
56'	
Free Movement	
35 MPH	
8 - 11 seconds	
3 lanes	
Both sides @ 8'	
10'	
--	
Sidewalk, 12' min.	
Tree wells, 5'	
Curb	
Trees at 30' o.c. Avg.	
None	

Recommended Adjustments:

- Decrease the bike lane to 5', each side.
- Decrease the parking lane to 8', each side.
- Increase the sidewalk to 7', each side.
- Add 5' tree wells @ 30' o.c., each side.
- Relocate power poles underground.
- Replace serpent-head street lights with single-head column street lights.



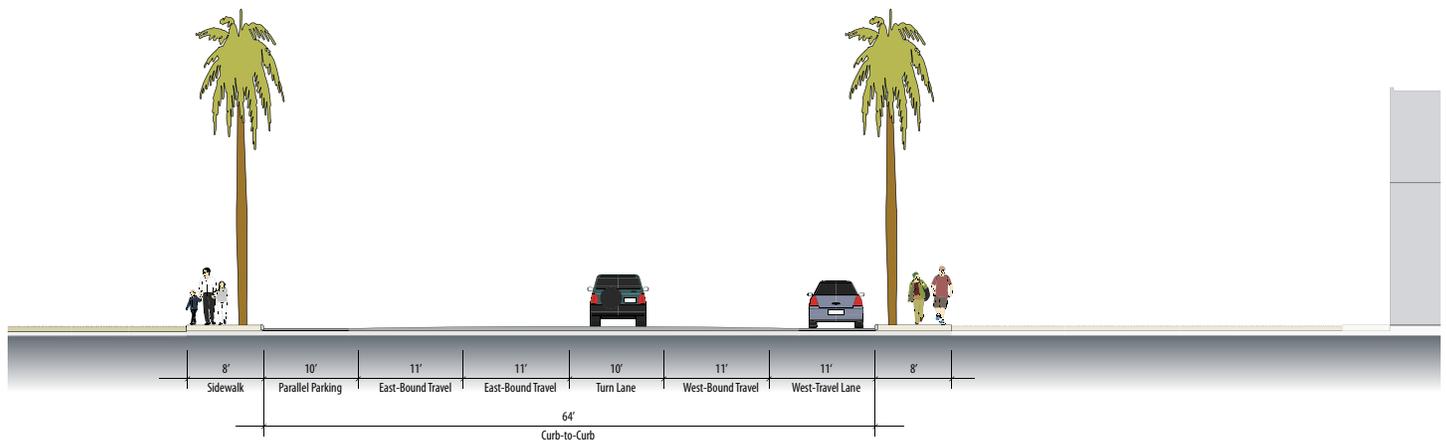
24M.208.031 Main Street - B

A. EXISTING CONDITION:

From approximately N. Catalina Street to Seaward Ave., Main Street is defined by the Ventura High School campus on the north and by nondescript one-story buildings on the south. This segment of Main Street represents the edge of the surrounding neighborhoods and has therefore been identified as a T4.5 Zone. The Ventura High School buildings are setback considerably from the right-of-way, given the size of the campus this creates a significant interruption in the rhythm of the Main Street frontage. The southern streetscape is defined by a combination of shopfronts, gas-station and vacant lots. The combination of frontage types along this segment of Main Street does not provide a unified or distinct character. Main Street is an urban thoroughfare with four travel lanes, a turning lane, and parallel parking on one side. The pedestrian experience is weak given that there are limited and inconsistent landscape elements, archaic serpent-head street lights and power poles.

Thoroughfare Type
Transect Zone Assignment
Right-of-Way Width
Pavement Width
Movement
Design Speed
Pedestrian Crossing Time
Traffic Lanes
Parking Lanes
Curb Radius
Public Frontage Type
Walkway Type
Planter Type
Curb Type
Landscape Type
Transportation Provision

Existing Main Street B	
Street	
T4.5	
80'	
64'	
Free Movement	
35 MPH	
8 - 11 seconds	
5 lanes	
One side @ 10'	
10'	
--	
Sidewalk, 8' min.	
Tree wells, 3'	
Curb	
Tree wells	
None	



B. PROPOSED APPROACH:

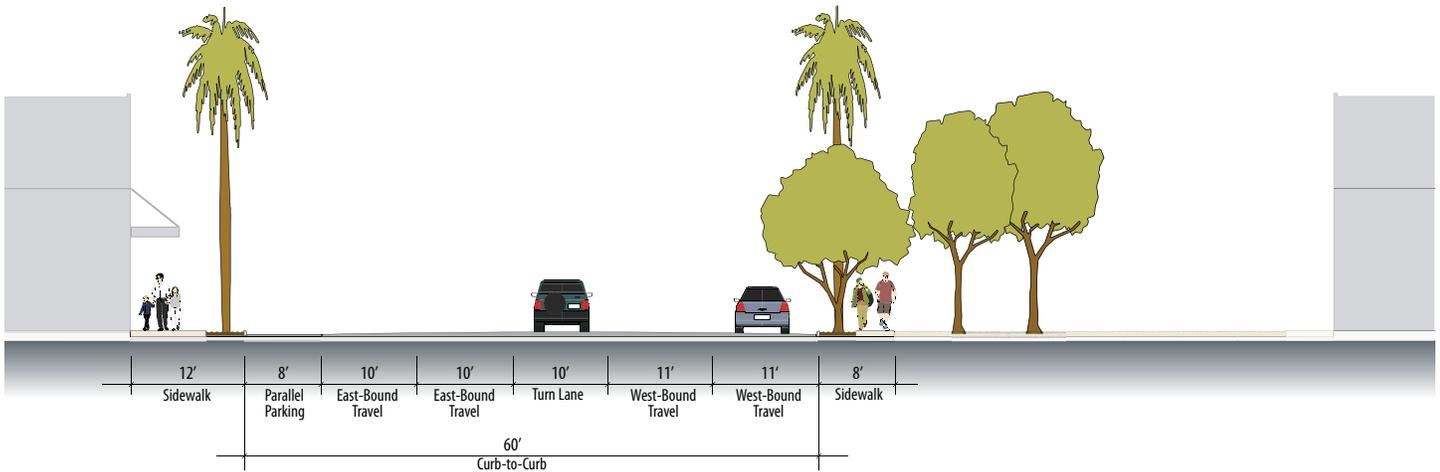
The proposed alterations are focused on establishing the street’s character and increasing the quality of the pedestrian experience. Future development on Main Street should establish a streetscape and complement the existing land use pattern. Buildings should not exceed 3 stories in height and, where appropriate, be provided with awnings that strengthen a sense of enclosure for shoppers and strollers. The pedestrian experience should be enhanced with sidewalks that have consistent street tree species and pedestrian-scale lighting. The existing on-street parallel parking is adequate to support the businesses along the southern edge of Main Street.

Recommended Adjustments:

- Decrease the parking lane to 8’, one side.
- Increase the sidewalk to 12’, one side.
- Add 5’ tree wells @ 30’ o.c., each side.
- Relocate power poles underground.
- Replace serpent-head street lights with single-head column street lights.

Thoroughfare Type	
Transect Zone Assignment	
Right-of-Way Width	
Pavement Width	
Movement	
Design Speed	
Pedestrian Crossing Time	
Traffic Lanes	
Parking Lanes	
Curb Radius	
Public Frontage Type	
Walkway Type	
Planter Type	
Curb Type	
Landscape Type	
Transportation Provision	

Proposed Main Street B	
Street	
T4.5	
80'	
56'	
Free Movement	
35 MPH	
8 - 11 seconds	
5 lanes	
One side @ 8'	
10'	
--	
Sidewalk, 12'/8' min.	
Tree wells, 5'	
Curb	
Trees at 30' o.c. Avg.	
None	



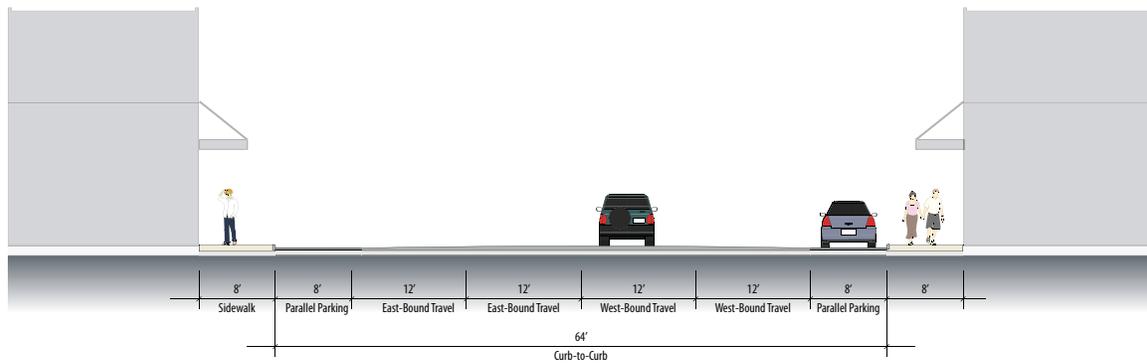
24M.208.032 Main Street - C

A. EXISTING CONDITION:

From approximately Seaward Avenue to Loma Vista Road, Main Street is defined by one and two-story buildings which are primarily of a commercial nature. This segment of Main Street represents the edge of the neighborhood and has therefore been identified as a T4.5 Zone. The northern and southern streetscapes are defined by shopfronts. It is an urban thoroughfare with four travel lanes and parallel parking each side. Landscape elements are limited and inconsistent.

Thoroughfare Type	
Transect Zone Assignment	
Right-of-Way Width	
Pavement Width	
Movement	
Design Speed	
Pedestrian Crossing Time	
Traffic Lanes	
Parking Lanes	
Curb Radius	
Public Frontage Type	
Walkway Type	
Planter Type	
Curb Type	
Landscape Type	
Transportation Provision	

Existing Main Street C	
Street	
T4.5	
80'	
64'	
Free Movement	
35 MPH	
8 - 11 seconds	
4 lanes	
Both sides @ 8'	
10'	
--	
Sidewalk, 8' min.	
None	
Curb	
Tree wells	
None	



B. PROPOSED APPROACH:

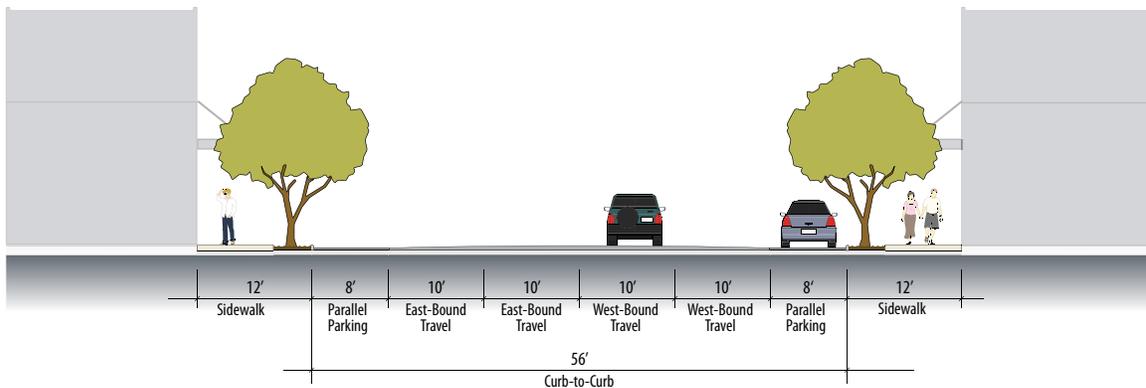
The proposed alterations are focused on reinforcing the existing street’s character and increasing the quality of the pedestrian experience. Main Street is almost exclusively a commercial street with a few mixed-use buildings. Future development on Main Street should maintain the established streetscape and land use pattern. Buildings should not exceed 3 stories in height and, where appropriate, be provided with awnings that strengthen a sense of enclosure for shoppers and strollers. The pedestrian experience should be enhanced with sidewalks that have street trees in wells and pedestrian-scale lighting. The existing on-street parallel parking is adequate to support the businesses along Main Street.

Recommended Adjustments:

- Decrease the travel lanes to 10’, each.
- Increase the sidewalk to 12’, each side.
- Add 5’ tree wells @ 30’ o.c., each side.
- Relocate power poles underground.
- Replace serpent-head street lights with single-head column street lights.

Thoroughfare Type
Transect Zone Assignment
Right-of-Way Width
Pavement Width
Movement
Design Speed
Pedestrian Crossing Time
Traffic Lanes
Parking Lanes
Curb Radius
Public Frontage Type
Walkway Type
Planter Type
Curb Type
Landscape Type
Transportation Provision

Proposed Main Street C	
Street	
T4.5	
80'	
56'	
Free Movement	
35 MPH	
8 - 11 seconds	
4 lanes	
Both sides @ 8'	
10'	
--	
Sidewalk, 12' min.	
Tree wells, 5'	
Curb	
Trees at 30' o.c. Avg.	
None	



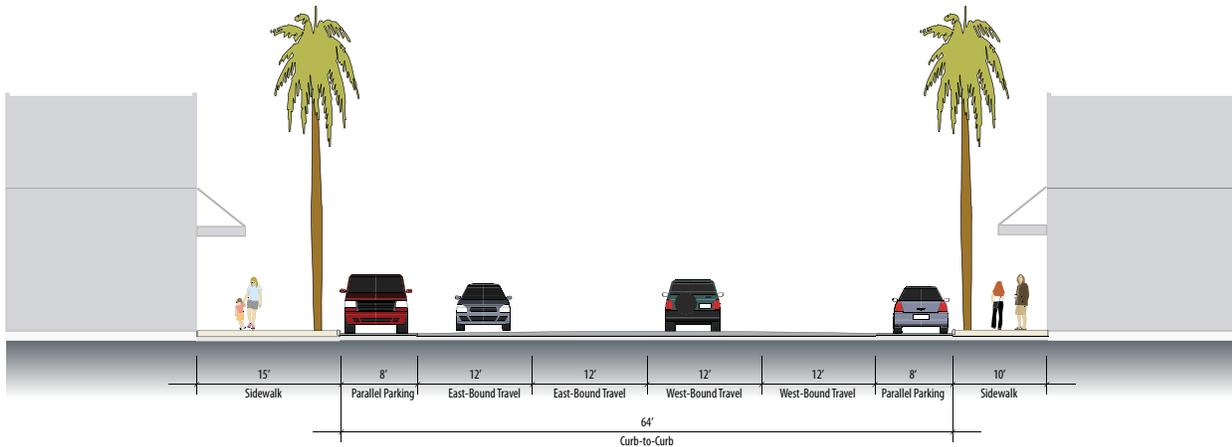
24M.208.033 Main Street - D

A. EXISTING CONDITION:

From approximately Loma Vista Road to Five Points is defined by one and two-story buildings which are entirely of a commercial nature. This segment of Main Street represents a neighborhood node and has been identified as a T5.2 Zone. The northern and southern streetscapes are defined by shopfronts. There are a significant number of probable redevelopment sites within this segment of Main Street. One of the most notable buildings that adds to the character of the streetscape is the Sears building that was converted into a warehouse shopping center. It is an urban thoroughfare with four travel lanes and parallel parking each side. Blank facades and vacant lots significantly degrade the pedestrian experience.

Thoroughfare Type	
Transect Zone Assignment	
Right-of-Way Width	
Pavement Width	
Movement	
Design Speed	
Pedestrian Crossing Time	
Traffic Lanes	
Parking Lanes	
Curb Radius	
Public Frontage Type	
Walkway Type	
Planter Type	
Curb Type	
Landscape Type	
Transportation Provision	

Existing Main Street D	
Street	
T5.2	
90'	
64'	
Free Movement	
35 MPH	
8 - 11 seconds	
4 lanes	
Both sides @ 8'	
10'	
--	
Sidewalk, 10' min.	
Tree well, 3'	
Curb	
Tree wells	
None	



B. PROPOSED APPROACH:

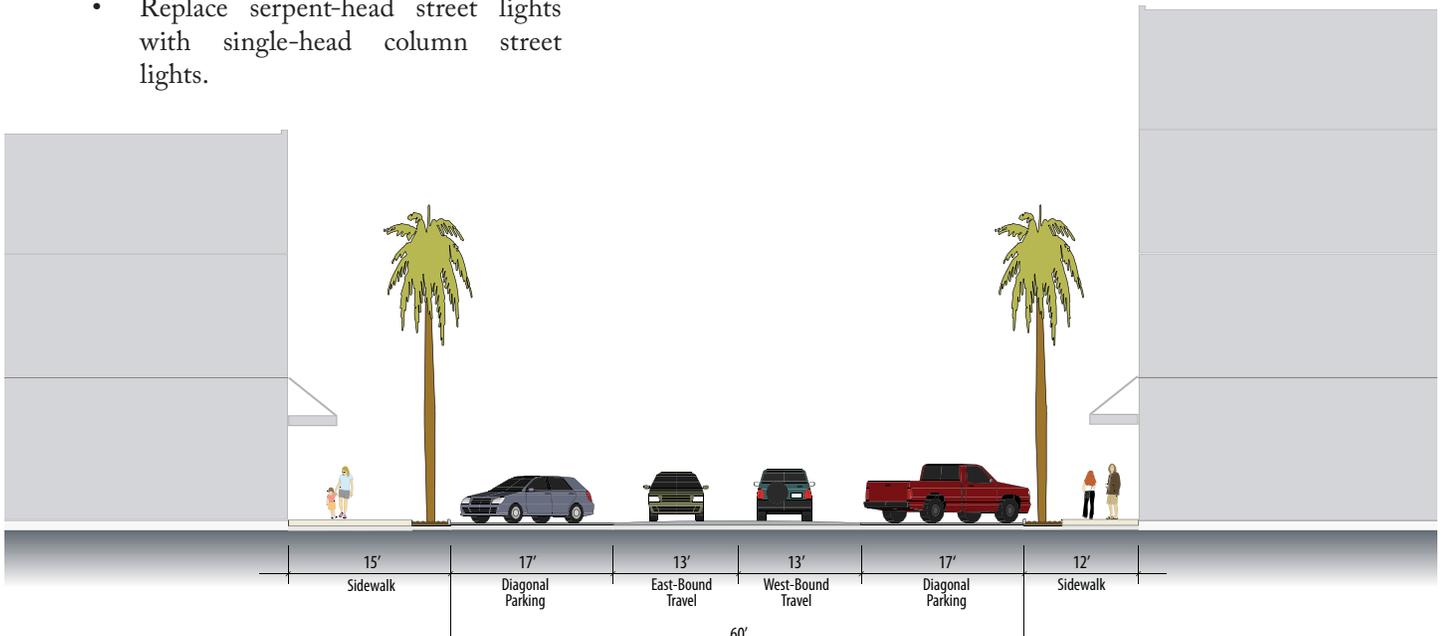
The proposed alterations are focused on reinforcing the existing street’s character and increasing the quality of the pedestrian experience. Main Street is almost exclusively a commercial street with a few mixed-use buildings. Future development on Main Street should maintain the established streetscape and land use pattern. Buildings should be at least 2 stories in height and, where appropriate, be provided with awnings that strengthen a sense of enclosure for shoppers and strollers. The pedestrian experience should be enhanced with sidewalks that have street trees in wells and pedestrian-scale lighting. The proposed diagonal parking helps to support the businesses along Main Street.

Recommended Adjustments:

- Remove two travel lanes.
- Add diagonal parking at 17’, each side.
- Increase the travel lanes to 13’, each side.
- Increase the sidewalk to 12’, each side.
- Add 5’ tree wells @ 30’ o.c., each side.
- Relocate power poles underground.
- Replace serpent-head street lights with single-head column street lights.

Thoroughfare Type	
Transect Zone Assignment	
Right-of-Way Width	
Pavement Width	
Movement	
Design Speed	
Pedestrian Crossing Time	
Traffic Lanes	
Parking Lanes	
Curb Radius	
Public Frontage Type	
Walkway Type	
Planter Type	
Curb Type	
Landscape Type	
Transportation Provision	

Proposed Main Street D	
Street	
T5.2	
80'	
60'	
Free Movement	
35 MPH	
8 - 11 seconds	
2 lanes	
Both sides @ 17'	
10'	
--	
Sidewalk, 12' min North side; 15' min. South side	
Tree wells, 5'	
Curb	
Trees at 30' o.c. Avg.	
None	



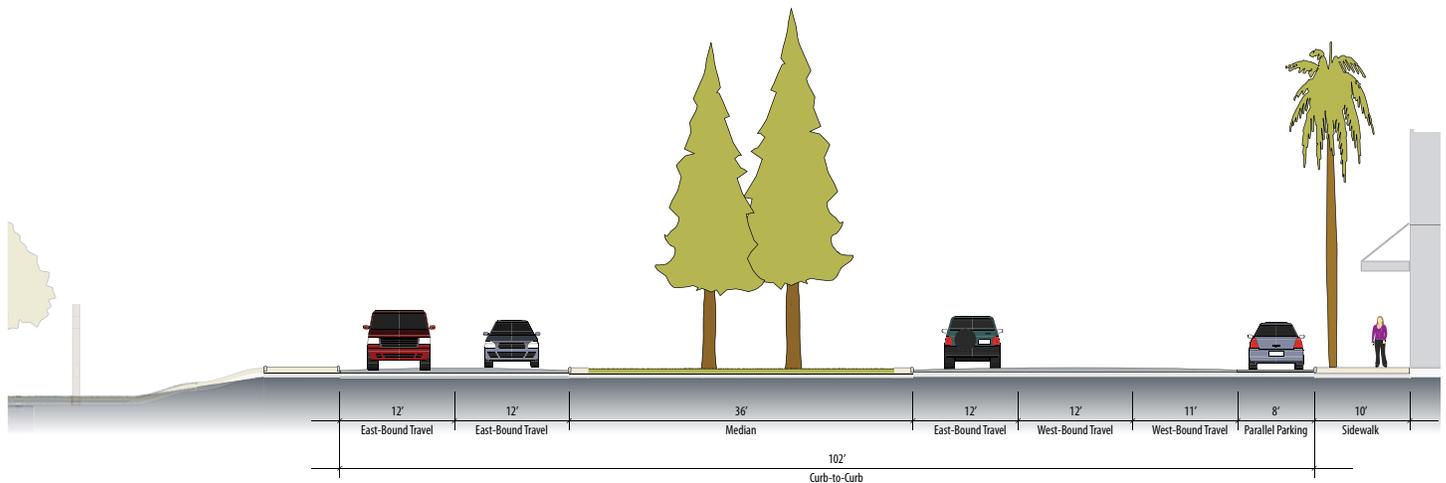
24M.208.034 Main Street - E

A. EXISTING CONDITION:

From approximately Five Points to Mills Road, Main Street is defined by one and two-story buildings commercial buildings on the north and one-story single family houses on the south. This segment of Main Street represents the edge of the neighborhood and has therefore been identified as a T4.5 Zone. The northern streetscape is defined by shopfronts and the southern streetscape is delineated with a masonry block wall. It is a highway that was converted to an urban thoroughfare, with five travel lanes and parallel parking on one side. Landscape elements are limited, inconsistent, and do not create a pleasant pedestrian experience.

Thoroughfare Type
Transect Zone Assignment
Right-of-Way Width
Pavement Width
Movement
Design Speed
Pedestrian Crossing Time
Traffic Lanes
Parking Lanes
Curb Radius
Public Frontage Type
Walkway Type
Planter Type
Curb Type
Landscape Type
Transportation Provision

Existing Main Street E	
Street	
T4.5	
120'	
102'	
Free Movement	
35 MPH	
8 - 11 seconds	
5 lanes	
One side @ 8'	
10'	
--	
Sidewalk, 10' min.	
None	
Curb	
Median	
None	



B. PROPOSED APPROACH:

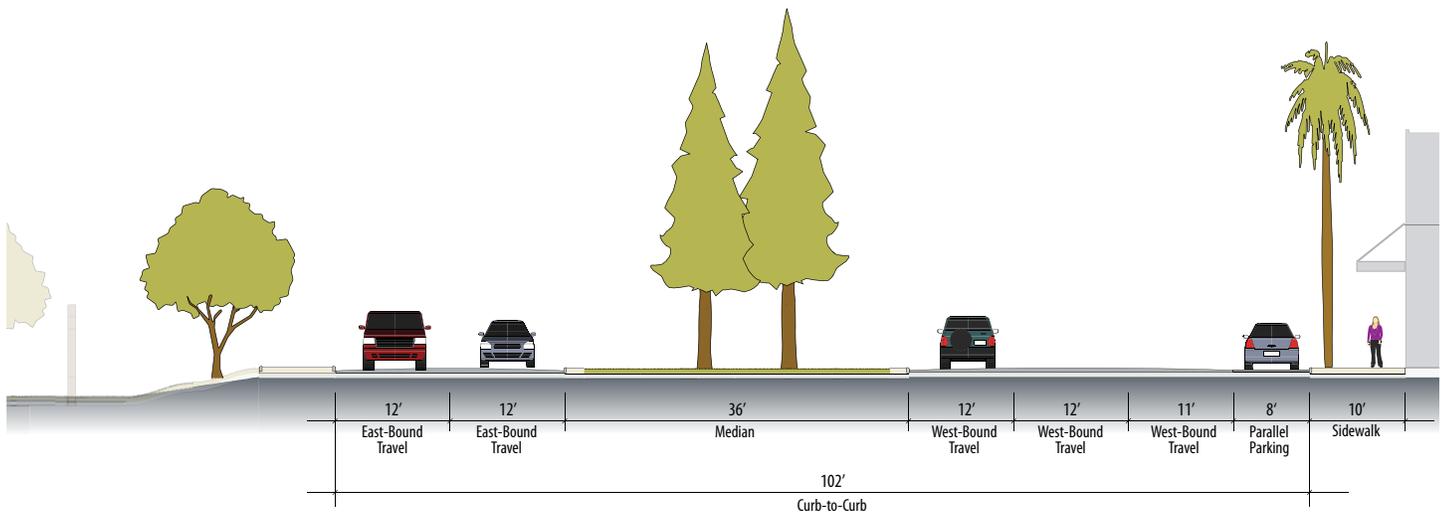
The proposed alterations are focused on mitigating the negative impacts of the existing thoroughfare’s highway character. The modifications intend to improve the quality of the pedestrian experience and enhance the accessibility of the existing businesses that front Main Street. Main Street is almost exclusively a commercial street with a few mixed-use buildings. Future development on Main Street should maintain the established streetscape and land use pattern. Buildings should not exceed 2 stories in height and, where appropriate, be provided with awnings that strengthen a sense of enclosure for shoppers and strollers. The pedestrian experience should be enhanced with sidewalks that have street trees in wells and pedestrian-scale lighting. The existing on-street parallel parking helps to support the businesses along Main Street.

Recommended Adjustments:

- Add 5’ tree wells @ 30’ o.c., each side.
- Relocate power poles underground.
- Replace serpent-head street lights with single-head column street lights.

Thoroughfare Type	
Transect Zone Assignment	
Right-of-Way Width	
Pavement Width	
Movement	
Design Speed	
Pedestrian Crossing Time	
Traffic Lanes	
Parking Lanes	
Curb Radius	
Public Frontage Type	
Walkway Type	
Planter Type	
Curb Type	
Landscape Type	
Transportation Provision	

Proposed Main Street E	
Street	
T4.5	
120'	
102'	
Free Movement	
35 MPH	
8 - 11 seconds	
2 lanes	
One side @ 8'	
10'	
--	
Sidewalk, 12' min.	
Median, Tree wells @ 30'o.c. avg	
Curb	
Trees at 30' o.c. Avg.	
None	



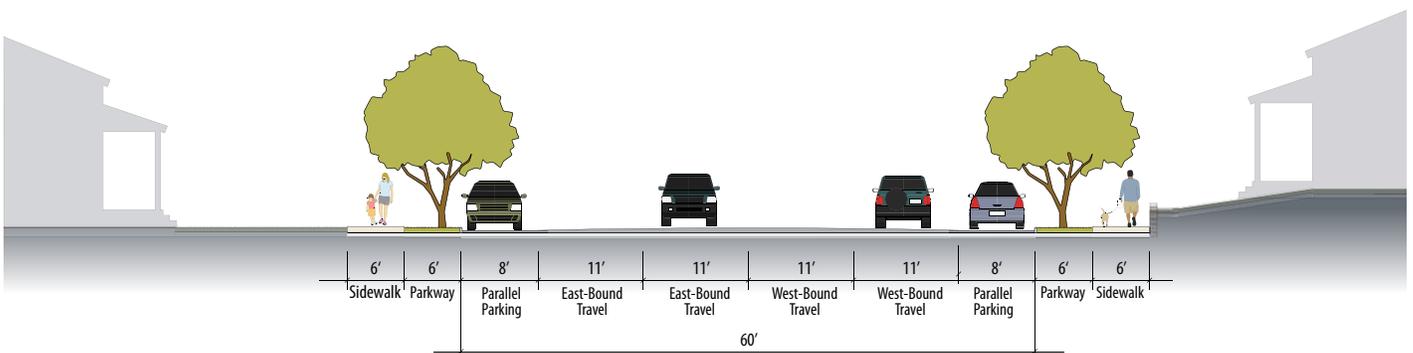
24M.208.040 Thompson Boulevard - A

A. EXISTING CONDITION:

From approximately Sanjon Road to Santa Cruz Street, Thompson Boulevard is defined by one and two-story buildings which are primarily of a residential character. Thompson Boulevard was once a highway but this segment has since been populated with a variety of single- and multi-family building types, though a few motorist oriented services still remain. This segment of Thompson Boulevard represents the edge of the surrounding neighborhood and has therefore been identified as a T4.5 Zone. The streetscape is primarily defined by residential frontage types with a few instances of commercial frontage types. Thompson Boulevard is an urban thoroughfare with four travel lanes and parking each side. Thompson Boulevard was designed as a highway with little consideration given to the pedestrian experience, this deficiency is mitigated by a parkway on each side.

Thoroughfare Type	
Transect Zone Assignment	
Right-of-Way Width	
Pavement Width	
Movement	
Design Speed	
Pedestrian Crossing Time	
Traffic Lanes	
Parking Lanes	
Curb Radius	
Public Frontage Type	
Walkway Type	
Planter Type	
Curb Type	
Landscape Type	
Transportation Provision	

Street
T4.5
84'
60'
Free Movement
35 MPH
8 - 11 seconds
4 lanes
Both sides @ 8'
10'
--
Sidewalk, 12' min.
Parkway
Curb
Trees at 30' o.c. Avg
None



B. PROPOSED APPROACH:

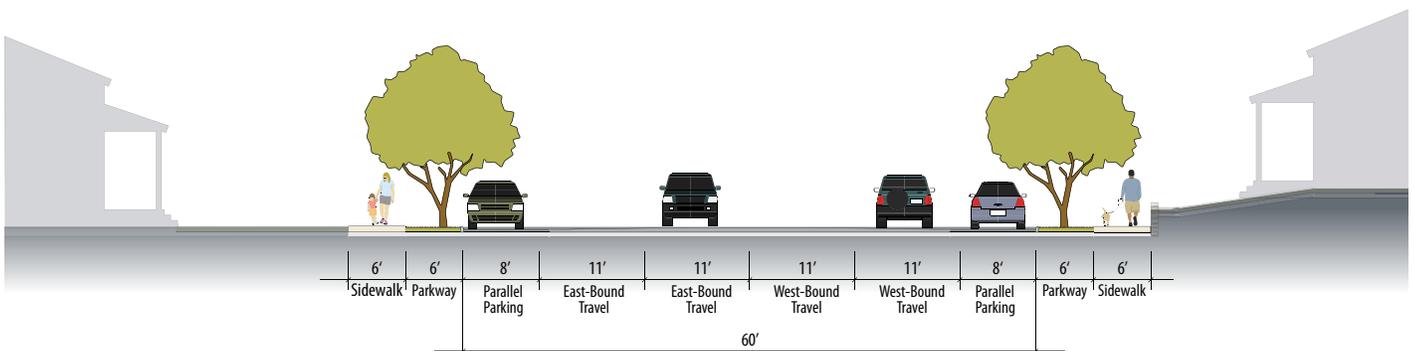
The proposed alterations are focused on reinforcing the street’s existing residential character and increasing the quality of the pedestrian experience. This segment of Thompson Boulevard is almost exclusively a residential street. Future development on Thompson Boulevard should maintain the established streetscape and land use pattern. Buildings should not exceed 2 stories in height and, where appropriate, be provided with awnings that strengthen a sense of enclosure for shoppers and strollers. The pedestrian experience should be enhanced with sidewalks that have street trees in wells and pedestrian-scale lighting. The existing on-street parallel parking helps to support the residences along Thompson Boulevard.

Recommended Adjustments:

- Relocate power poles underground.
- Replace serpent-head street lights with single-head column street lights.

Thoroughfare Type
Transect Zone Assignment
Right-of-Way Width
Pavement Width
Movement
Design Speed
Pedestrian Crossing Time
Traffic Lanes
Parking Lanes
Curb Radius
Public Frontage Type
Walkway Type
Planter Type
Curb Type
Landscape Type
Transportation Provision

Proposed Thompson Boulevard A	
Street	
T4.5	
84'	
60'	
Free Movement	
35 MPH	
8 - 11 seconds	
4 lanes	
Both sides @ 8'	
10'	
--	
Sidewalk, 12' min.	
Parkway	
Curb	
Trees at 30' o.c. Avg.	
None	



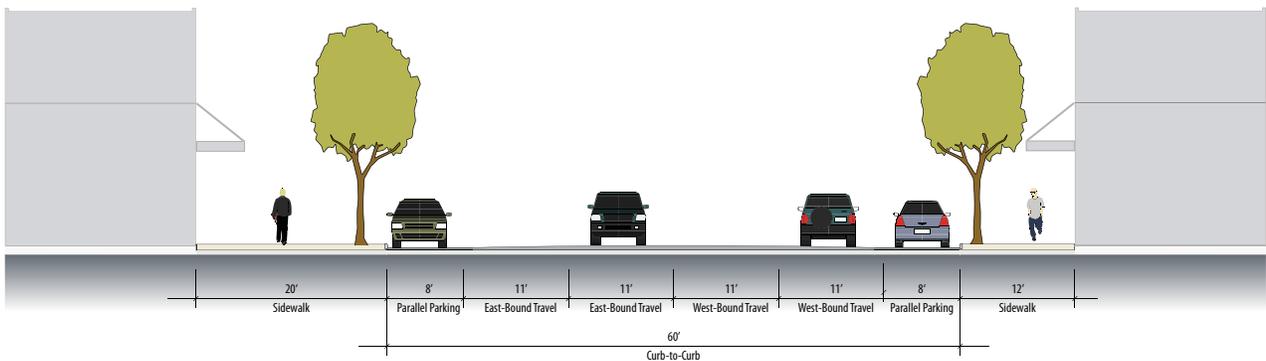
24M.208.041 Thompson Boulevard - B

A. EXISTING CONDITION:

From approximately Santa Cruz Street to Seaward Avenue, Thompson Boulevard is defined by one and two-story buildings which are primarily of a commercial nature. There are a number of underutilized and neglected sites along Thompson Boulevard. Many of the businesses are not currently oriented to serving the adjacent neighborhood. This segment of Thompson Boulevard represents the edge of the surrounding neighborhood and has therefore been identified as a T4.5 Zone. The streetscape is primarily defined by commercial frontage types. Thompson Boulevard is an urban thoroughfare with four travel lanes and parking each side. Sidewalks and landscape elements are inconsistent.

Thoroughfare Type	
Transect Zone Assignment	
Right-of-Way Width	
Pavement Width	
Movement	
Design Speed	
Pedestrian Crossing Time	
Traffic Lanes	
Parking Lanes	
Curb Radius	
Public Frontage Type	
Walkway Type	
Planter Type	
Curb Type	
Landscape Type	
Transportation Provision	

Existing Thompson Boulevard B	
Street	
T4.5	
92'	
60'	
Free Movement	
35 MPH	
8 - 11 seconds	
4 lanes	
Both sides @ 8'	
10'	
--	
Sidewalk, 12' min.	
Tree wells	
Curb	
Trees at 30' o.c. Avg.	
None	



B. PROPOSED APPROACH:

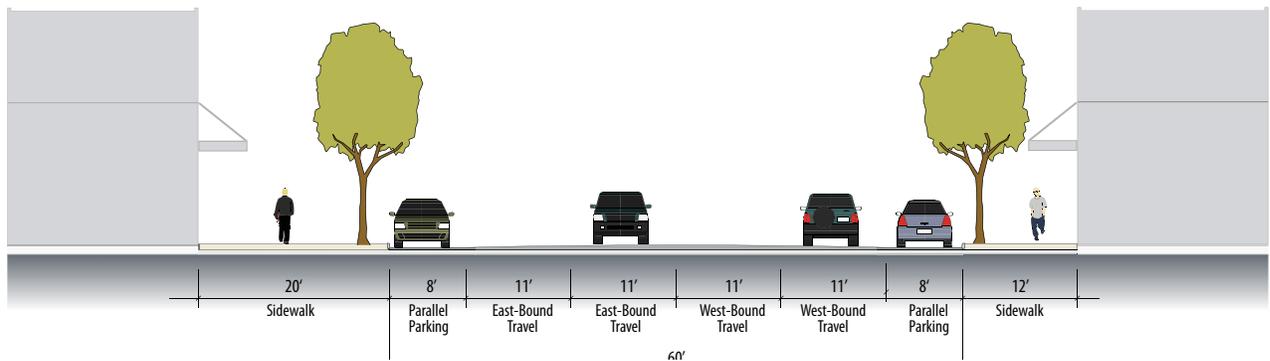
The proposed alterations are focused on reinforcing the existing street’s character, increasing the quality of the pedestrian experience. Thompson Boulevard is almost exclusively a commercial street with a few mixed-use buildings. Future development on Thompson Boulevard should maintain the established streetscape and land use pattern. Buildings should not exceed 2 stories in height and, where appropriate, be provided with awnings that strengthen a sense of enclosure for shoppers and strollers. The pedestrian experience should be enhanced with sidewalks that have street trees in wells and pedestrian-scale lighting. The existing on-street

Thoroughfare Type	
Transect Zone Assignment	
Right-of-Way Width	
Pavement Width	
Movement	
Design Speed	
Pedestrian Crossing Time	
Traffic Lanes	
Parking Lanes	
Curb Radius	
Public Frontage Type	
Walkway Type	
Planter Type	
Curb Type	
Landscape Type	
Transportation Provision	

Proposed Thompson Boulevard B	
Street	
T4.5	
92'	
60'	
Free Movement	
35 MPH	
8 - 11 seconds	
4 lanes	
Both sides @ 8'	
10'	
--	
Sidewalk, 12' min.	
Tree wells, 5'	
Curb	
Trees at 30' o.c. Avg.	
None	

Recommended Adjustments:

- Add 5' tree wells @ 30' o.c., each side.
- Relocate power poles underground.
- Replace serpent-head street lights with single-head column street lights.



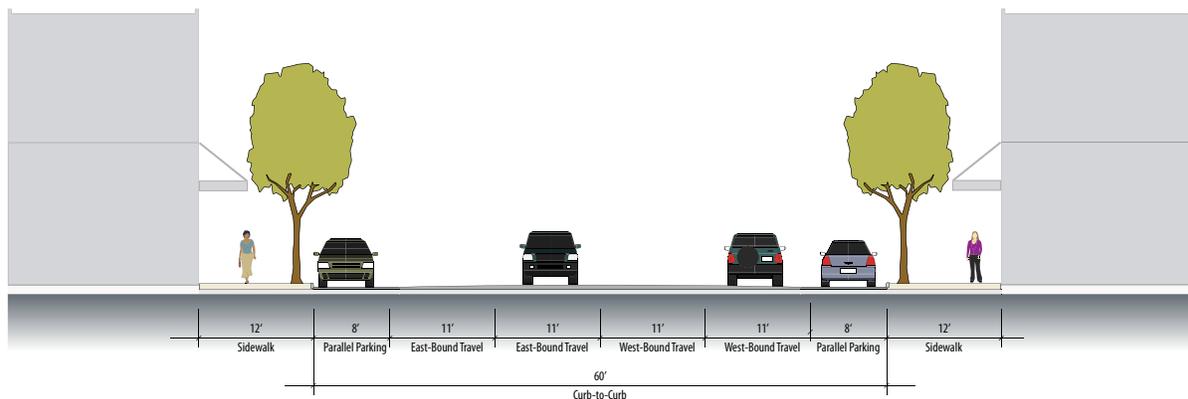
24M.208.042 Thompson Boulevard - C

A. EXISTING CONDITION:

From approximately Seaward Avenue to Howard Street, Thompson Boulevard is defined by one-story buildings which are primarily of a commercial nature. There are a number of underutilized and neglected sites along Thompson Boulevard. Many of the businesses are not currently oriented to serving the adjacent neighborhood. This segment of Thompson Boulevard represents the edge of the surrounding neighborhood and has therefore been identified as a T4.5 Zone. The streetscape is primarily defined by commercial frontage types. Thompson Boulevard is an urban thoroughfare with four travel lanes and parking each side. Sidewalks and landscape elements are inconsistent.

Thoroughfare Type	
Transect Zone Assignment	
Right-of-Way Width	
Pavement Width	
Movement	
Design Speed	
Pedestrian Crossing Time	
Traffic Lanes	
Parking Lanes	
Curb Radius	
Public Frontage Type	
Walkway Type	
Planter Type	
Curb Type	
Landscape Type	
Transportation Provision	

Existing Thompson Boulevard C	
Street	
T4.5	
84'	
60'	
Free Movement	
35 MPH	
8 - 11 seconds	
4 lanes	
Both sides @ 8'	
10'	
--	
Sidewalk, 12' min.	
Tree wells	
Curb	
Trees at 30' o.c. Avg.	
None	



B. PROPOSED APPROACH:

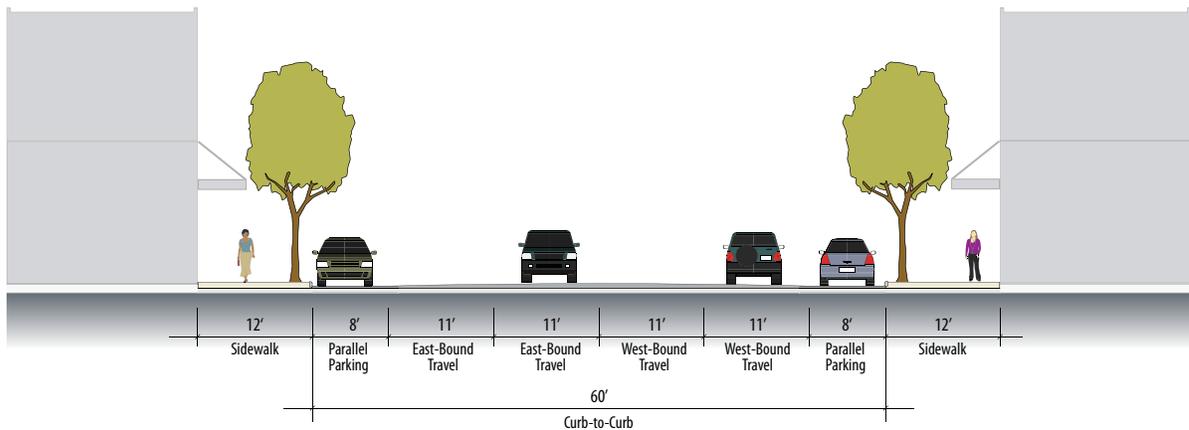
The proposed alterations are focused on reinforcing the existing street’s character and increasing the quality of the pedestrian experience. Thompson Boulevard is almost exclusively a commercial street with a few mixed-use buildings. Future development on Thompson Boulevard should maintain the established streetscape and land use pattern. Buildings should not exceed 3 stories in height and, where appropriate, be provided with awnings that strengthen a sense of enclosure for shoppers and strollers. The pedestrian experience should be enhanced with sidewalks that have street trees in wells and pedestrian-scale lighting. The existing on-street parallel parking helps to support the businesses along Thompson Boulevard.

Recommended Adjustments:

- Add 5’ tree wells @ 30’ o.c., each side.
- Relocate power poles underground.
- Replace serpent-head street lights with single-head column street lights.

Thoroughfare Type	
Transect Zone Assignment	
Right-of-Way Width	
Pavement Width	
Movement	
Design Speed	
Pedestrian Crossing Time	
Traffic Lanes	
Parking Lanes	
Curb Radius	
Public Frontage Type	
Walkway Type	
Planter Type	
Curb Type	
Landscape Type	
Transportation Provision	

Proposed Thompson Boulevard C	
Street	
T4.5	
84'	
60'	
Free Movement	
35 MPH	
8 - 11 seconds	
4 lanes	
Both sides @ 8'	
10'	
--	
Sidewalk, 12' min.	
Tree wells, 5'	
Curb	
Trees at 30' o.c. Avg.	
None	



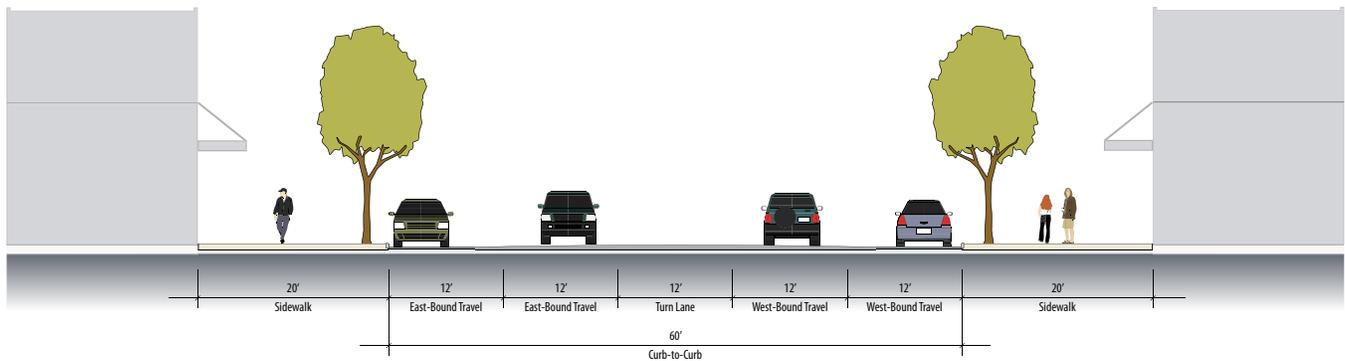
24M.208.043 Thompson Boulevard - D

A. EXISTING CONDITION:

From approximately Howard Street to Main Street, Thompson Boulevard is defined by one-story buildings which are commercial in nature and sit on relatively large sites. There is no cohesive or distinct character among this segment of Thompson Boulevard. Many of the businesses are not currently oriented to serving the adjacent neighborhood. This segment of Thompson Boulevard represents the edge of the surrounding neighborhood and has therefore been identified as a T4.5 Zone. There are large gaps in the streetscape, which is not defined on either side of the street. Thompson Boulevard is an urban thoroughfare with four travel lanes and parking each side. Sidewalks and landscape elements are inconsistent.

Thoroughfare Type	
Transect Zone Assignment	
Right-of-Way Width	
Pavement Width	
Movement	
Design Speed	
Pedestrian Crossing Time	
Traffic Lanes	
Parking Lanes	
Curb Radius	
Public Frontage Type	
Walkway Type	
Planter Type	
Curb Type	
Landscape Type	
Transportation Provision	

Street
T4.5
100'
60'
Free Movement
35 MPH
8 - 11 seconds
5 lanes
None
10'
--
Sidewalk, 20' min.
Tree wells
Curb
Trees, spacing varies
None



B. PROPOSED APPROACH:

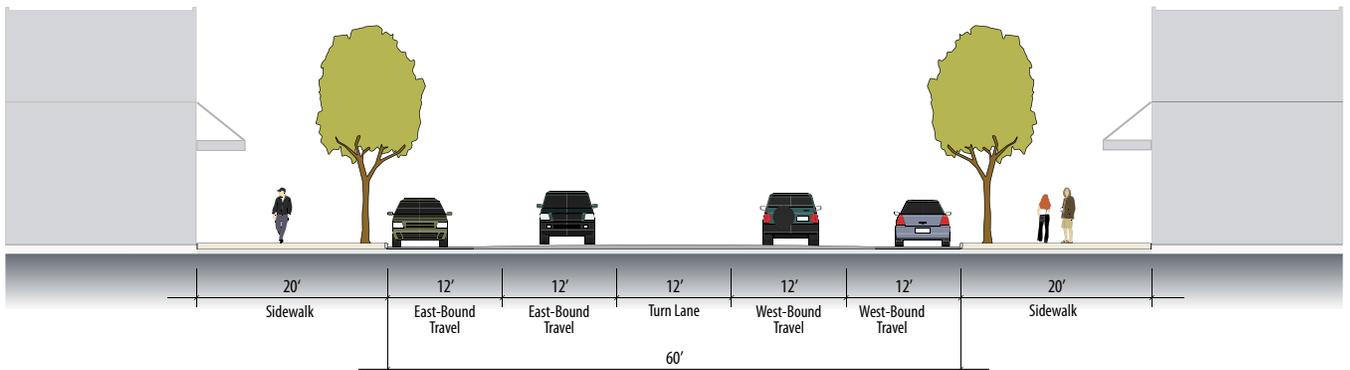
The proposed alterations are focused on reinforcing the existing street's character and increasing the quality of the pedestrian experience. Thompson Boulevard is almost exclusively a commercial street with a few mixed-use buildings. Future development on Thompson Boulevard should maintain the established streetscape and land use pattern. Where appropriate, buildings should be provided with awnings that strengthen a sense of enclosure for shoppers and strollers. The pedestrian experience should be enhanced with sidewalks that have street trees in wells and pedestrian-scale lighting. The existing on-street parallel parking is adequate to support the businesses along Thompson Boulevard.

Recommended Adjustments:

- Add 5' tree wells @ 30' o.c., each side.
- Relocate power poles underground.
- Replace serpent-head street lights with single-head column street lights.

Thoroughfare Type	
Transect Zone Assignment	
Right-of-Way Width	
Pavement Width	
Movement	
Design Speed	
Pedestrian Crossing Time	
Traffic Lanes	
Parking Lanes	
Curb Radius	
Public Frontage Type	
Walkway Type	
Planter Type	
Curb Type	
Landscape Type	
Transportation Provision	

Proposed Thompson Boulevard D	
Street	
T4.5	
100'	
60'	
Free Movement	
35 MPH	
8 - 11 seconds	
5 lanes	
None	
10'	
--	
Sidewalk, 20' min.	
Tree wells, 5'	
Curb	
Trees at 30' o.c. Avg.	
None	



24M.208.200 Public Frontages

TABLE D-1: Public Frontages - General

The **Public Frontage** is the area between the private lot line and the edge of the vehicular lanes. Dimensions are given in TableD-2 (Public Frontages - Specific)

		PLAN			
		LOT	R.O.W.		
		PRIVATE FRONTAGE	PUBLIC FRONTAGE		
<p>a. (HW) For Highways: This frontage has open swales drained by percolation, bicycle trails and no parking. The landscaping consists of the natural condition or multiple species arrayed in naturalistic clusters. Buildings are buffered by distance or berms.</p>		<p>T1 T2 T3</p>			
<p>b. (RR) For Rural Roads: This frontage has open swales drained by percolation, without parking. The landscaping consists of multiple tree and shrub species arrayed in naturalistic clusters</p>		<p>T1 T2 T3</p>			
<p>c. (SR) For Standard Roads: This frontage has open swales drained by percolation and a walking path or bicycle trail along one or both sides and yield parking. The landscaping consists of multiple species arrayed in naturalistic clusters.</p>		<p>T3 T4</p>			
<p>d. (RS) For Residential Street: This frontage has raised curbs drained by inlets and narrow sidewalks separated from the vehicular lanes by a wide continuous planter, with parking on one or both sides. The landscaping consists of street trees of a single or alternating species aligned in a regularly spaced alley.</p>		<p>T3 T4</p>			
<p>e. (SS) (AV) For Standard Streets or Avenues: This frontage has raised curbs drained by inlets and wide sidewalks separated from the vehicular lanes by a narrow continuous planter with parking on both sides. The landscaping consists of a single tree species aligned in a regularly spaced alley.</p>		<p>T5 T6</p>			
<p>f. (CS) (AV) For Commercial Streets or Avenues: This frontage has raised curbs drained by inlets and very wide sidewalks along both sides separated from the vehicular lanes by separate tree wells with grates and parking on both sides. The landscaping consists of a single tree species aligned with regular spacing where possible but clears the shopfront entrances.</p>		<p>T5 T6</p>			
<p>g. (BV) For Boulevards: This frontage has slip roads on both sides. It consists of raised curbs drained by inlets and sidewalks along both sides, separated from the vehicular lanes by planters. The landscaping consists of double rows of a single tree species aligned in a regularly spaced alley.</p>		<p>T3 T4 T5 T6</p>			

Table D-2: Public Frontages - Specific. This table assembles prescriptions and dimensions for the public frontage elements - curbs, walkways and planters – relative to specific thoroughfare types within Transect Zones. Locally appropriate planting species should be filled in to the calibrated Code.

		R U R A L I I I I I I I I I I I I I I I I T R A N S E C T I I I I I I I I I I I I I I I I U R B A N													
TRANSECT ZONE		T1 T2 T3			T1 T2 T3			T3 T4		T4 T5		T5 T6		T5 T6	
Public Frontage Type		HW & RR			RR & SR			RS		RS-SS-AV		RS-CS-AV-BV		CS-AV-BV	
<p>a. Assembly: The principal variables are the type and dimension of curbs, walkways, planters and landscape.</p>	Total Width														
		16-24 feet			12-24 feet			12-18 feet		12-18 feet		18-24 feet		18-30 feet	
<p>b. Curb: The detailing of the edge of the vehicular pavement, incorporating drainage.</p>	Type														
	Radius	Open Swale 10-30 feet			Open Swale 10-30 feet			Raised Curb 5-20 feet							
<p>c. Walkway: The pavement dedicated exclusively to pedestrian activity.</p>	Type														
	Width	Path Optional n/a			Path 4-8 feet			Sidewalk 4-8 feet		Sidewalk 4-8 feet		Sidewalk 12-20 feet		Sidewalk 12-30 feet	
<p>d. Planter: The layer which accommodates street trees and other landscape.</p>	Arrangement														
	Species	Clustered Multiple			Clustered Multiple			Regular Alternating		Regular Single		Regular Single		Opportunistic Single	
	Planter Type	Continuous Swale			Continuous Swale			Continuous Planter		Continuous Planter		Continuous Planter		Tree Well	
	Planter Width	8 feet-16 feet			8 feet-16 feet			8 feet-12 feet		8 feet-12 feet		4 feet-6 feet		4 feet-6 feet	



The conceptual photo-simulation shows one possible scenario of incremental improvements at the Main Street and Santa Rosa Street intersection possible under the Midtown Corridors Development Code.