

Saticoy & Wells  
COMMUNITY  
PLAN

CITY OF  
VENTURA

ACHIEVING THE VISION OF THE

ventura general plan

ADOPTED • NOVEMBER 2009



City of San Buenaventura

Saticoy & Wells Community Plan

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To most efficiently coordinate new infill and existing development, portions of the Saticoy and Wells Planning Communities were excluded and other areas re-combined into the Saticoy & Wells Community Plan Area (see Figure 11-1). As shown on Figure 11-1, the portions of the Saticoy and Wells Planning Communities not covered by this Plan include lands designated as Agriculture, thus subject to Save Our Agricultural Resources (SOAR) Ordinance, while other areas are already developed and established residential neighborhoods. For these reasons they have been omitted.

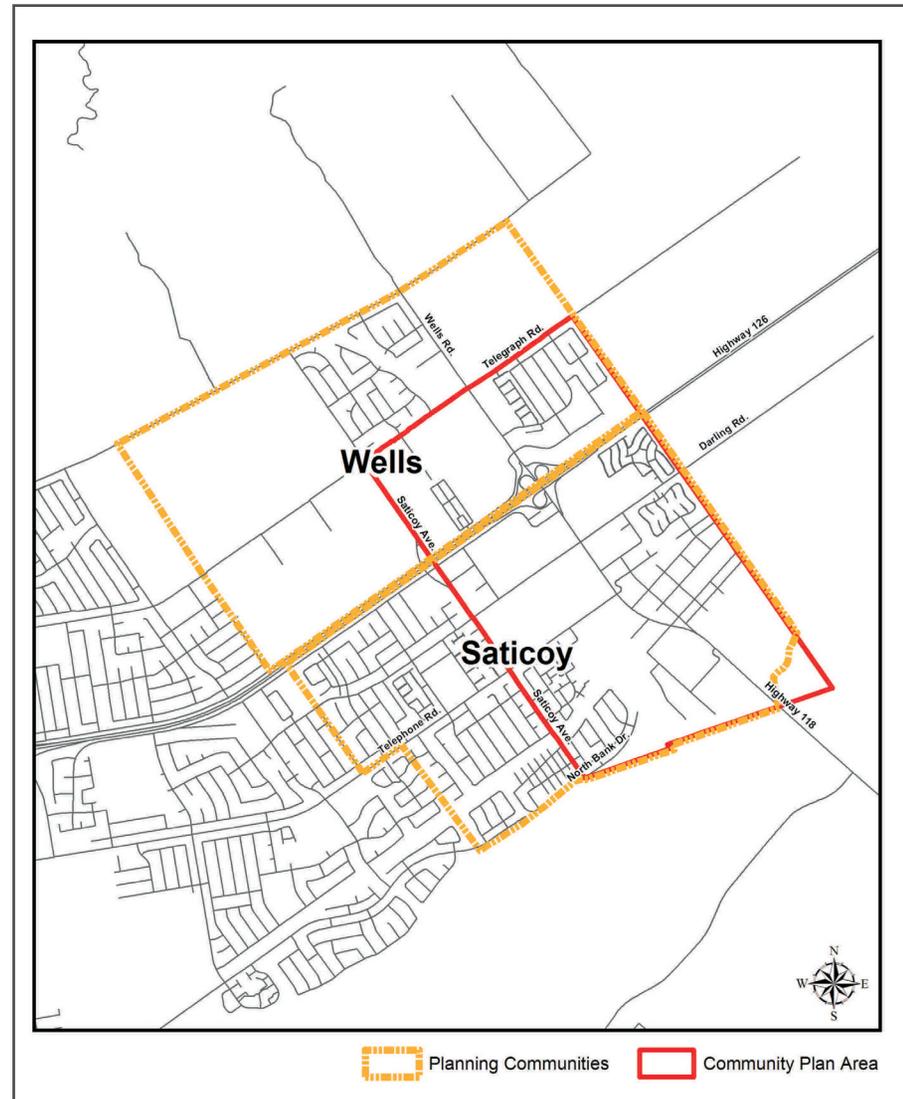


Figure 11-1 - Saticoy & Wells Planning Communities & Community Plan Area

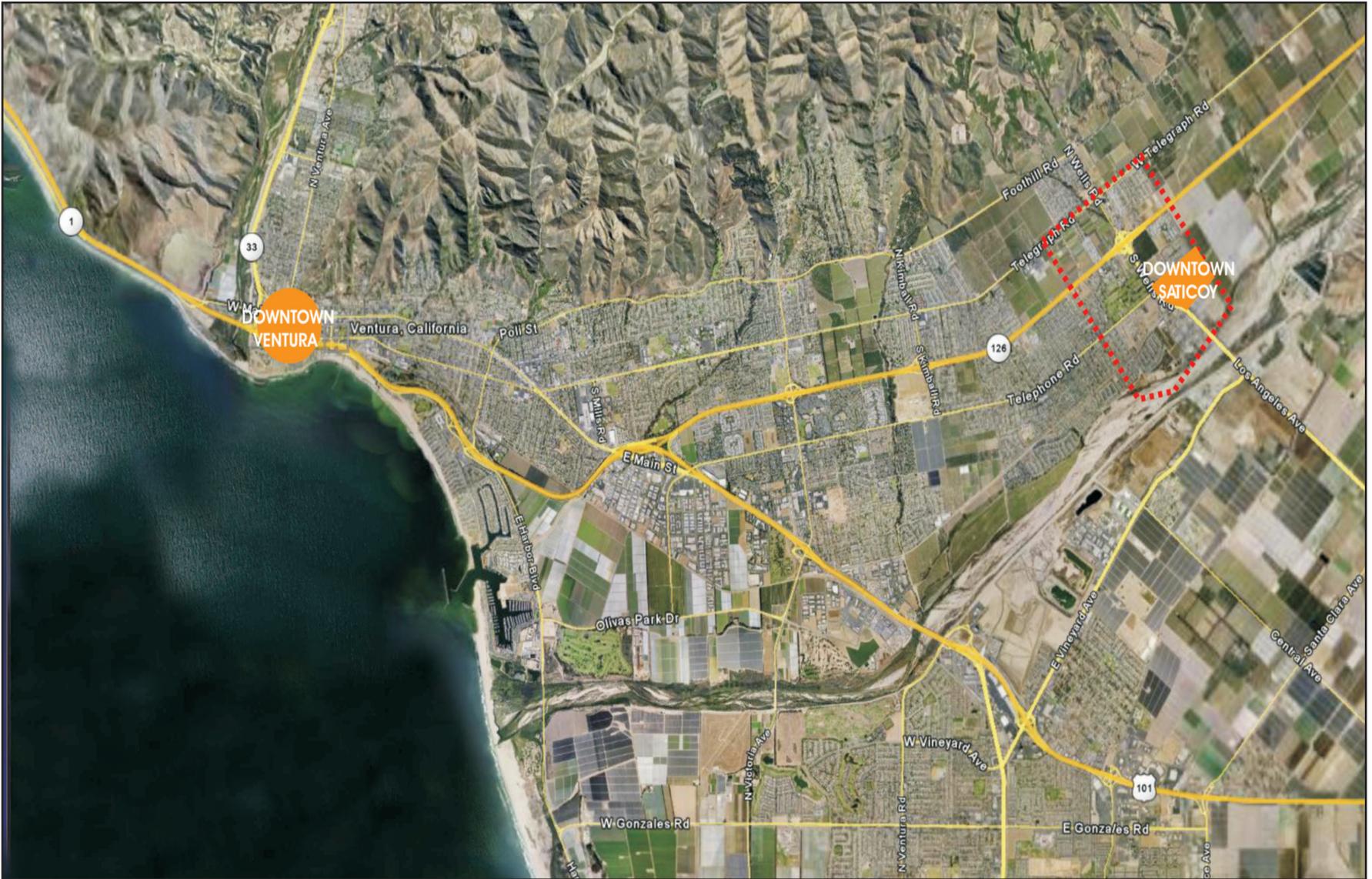


Figure 11-2 - Regional Location of Community Plan Area



## Chapter 11

# SATICOY & WELLS COMMUNITY PLAN

## INTRODUCTION

**Our goal is to create six distinct, yet interconnected, walkable neighborhoods that improve over time by requiring well-designed development, thoroughfares usable by all modes of transportation, and providing neighborhood amenities that meet the unique needs of the Saticoy and Wells communities.**

### Setting

The Saticoy & Wells Community Plan Area consists of approximately 1,000 acres, located at the easterly edge of the City, bounded by the Santa Clara River on the south, Telegraph Road to the north, Saticoy Avenue on the west, and the Franklin-Wason Barranca to the east (Figure 11-3). The Plan Area is divided by Highway 126 and Highway 118 (Wells Road), as well as the Santa Paula Branch Line (railroad).

Downtown Ventura is approximately 10 miles to the west, and Santa Barbara is 36 miles north. (See Figure 11-2) Highway 118 connects Ventura with the easterly portion of the City of Oxnard and continues east through productive agricultural fields to the City of Moorpark and the Los Angeles metropolitan area. State Highway 126 continues east through the Santa Clara River Valley.

### History

The town of Saticoy was laid out in the late 1800's

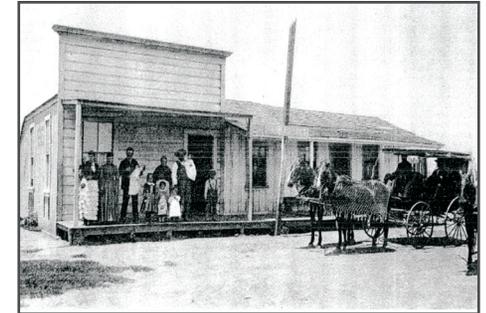
on land that was previously inhabited by the thriving Chumash civilization, which lived in the Saticoy and Wells areas and along the Pacific Coast for 9,000 years. There were actually two towns known as Saticoy. One was referred to as West Saticoy and the other, Lower Saticoy. The two Saticoy's are today known to be the area of Old Town Saticoy, and the other is located just north of Darling Road, and east of Saticoy Avenue.

In 1887 the Santa Clarita Rail Line was constructed, connecting Saticoy to the main San Francisco-Los Angeles line of the Southern Pacific. Saticoy town was the center of the area's citrus, bean, and produce, and the rail line facilitated this activity. The rail stop brought both passenger and freight, but passenger service eventually stopped in 1934. From that point on, the rail line was used only for local freight such as produce, packing and building supplies, and consumer goods. Today, only the line between Montalvo and Piru is active and is used mostly for inbound freight and the occasional shipment of agricultural products.

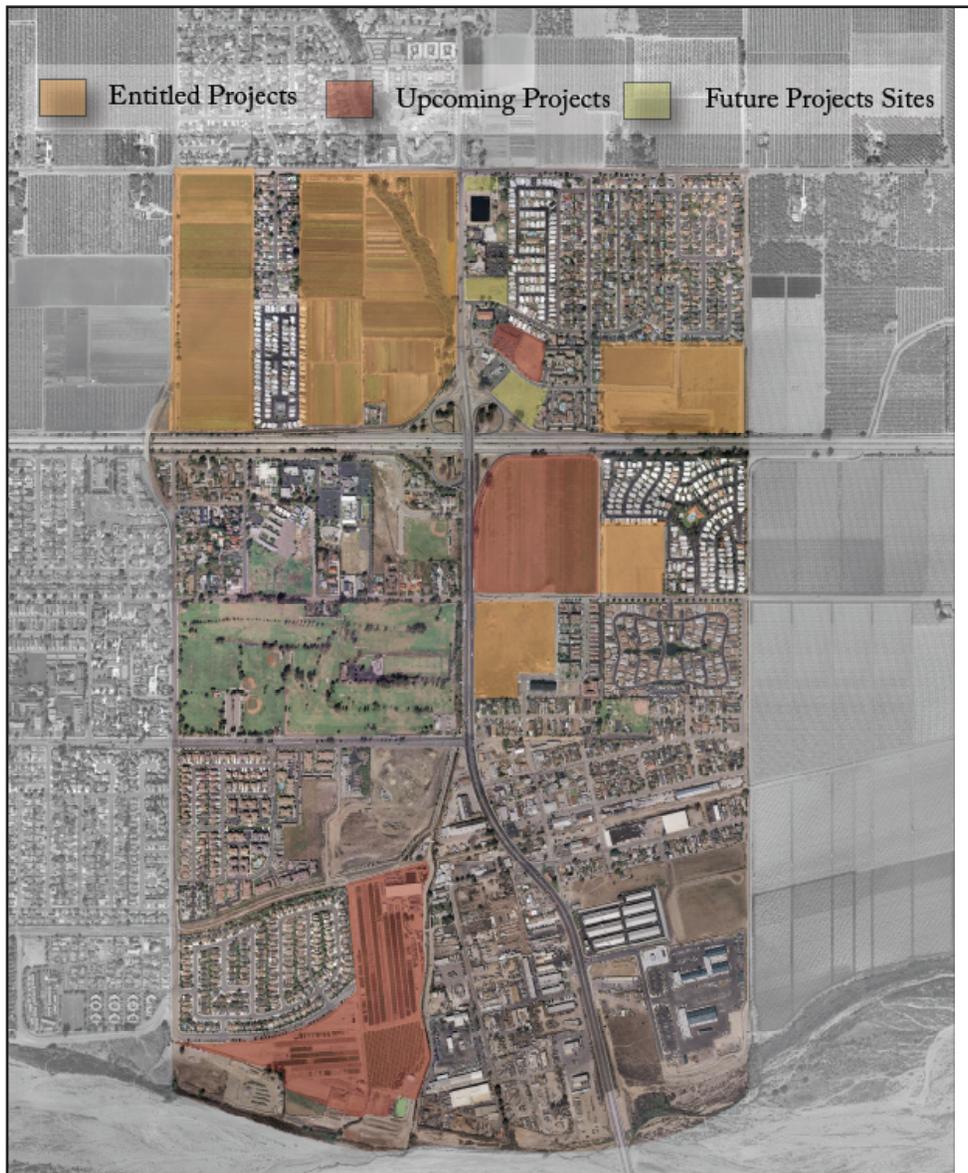
Since the 1800's, Saticoy has changed from a small agricultural center along the banks of the Santa Clara River into a larger and more complex residential, commercial, and industrial area. In the late 1890's through the 1920's, the area significantly contributed to the growth of Ventura as the center of citrus, beans and other crops. During this time, farmers used the historic Southern Pacific train depot to load and ship crops to other cities and states.

### Context and Character

Immediately north and south of the railroad tracks in Old Town Saticoy, a continuous rectilinear network of north-south and east-west streets generates a clear pattern of blocks. However, this pattern quickly



Saticoy in the Late 1800s



**Figure 11-3 - Development Opportunities**

dissolves into a disconnected block-street network. For instance, Los Angeles Avenue and Alelia Avenue dead end into a berm that runs east of Telegraph Road. And, at Telegraph Road, the grid rotates and Telephone runs north to meet Astor Street, instead of connecting through.

Today, various housing tracts designed with no relationship to each other display typical sprawl patterns with disconnected, wide streets and cul-de-sacs, introverted open spaces, front-loaded garages, and the repetition of a single housing type. These developments represent a stark contrast to the interconnected grid of Old Town Saticoy and reveal the dramatic shift the Plan Area has undergone in the last half century.

As of December 2006 there were over eight major development projects in various stages of the entitlement process in the Saticoy and Wells Plan Area. Recognizing this unique opportunity, the City of Ventura saw the need for a comprehensive community plan to bring together these individual infill developments and relate them to a larger community vision. (Figure 11- 3)

**Jurisdiction**

Approximately 435 acres in the Plan Area are under Ventura County jurisdiction, but lie within the City of Ventura’s Sphere of Influence. Unincorporated land within the Sphere of Influence is the responsibility of Ventura County, however all projects are reviewed by the City of Ventura.

The *Guidelines for Orderly Development* have been adopted by Ventura County, all cities within the County, and the Local Agency Formation Commission. They provide a framework for intergovernmental cooperation

and specify that urban development should be located within cities whenever and wherever practical. To achieve the growth potential within the unincorporated areas, the City needs to coordinate closely with the County to prepare economic development strategies and the infrastructure necessary to accommodate growth. However, unincorporated areas may not receive City services until annexed into the City.

The City will seek joint adoption of this Plan with the County of Ventura to ensure that the community goals for Old Town Saticoy are implemented across both jurisdictions. Due to the involved process of annexing these areas, the City believes the most effective means of ensuring that goals are met is through joint adoption until such time annexation occurs. And until joint adoption occurs, the City will heighten its review of private projects occurring in the unincorporated area.

Because this Community Plan is intended to implement the City's General Plan at the neighborhood level, its goal, policies, and actions are consistent with the General Plan. While this Community Plan provides further direction on requirements for new development policies and actions of the General Plan remain applicable and in effect.

**Plan Preparation Process**

Civic engagement is critical to successful planning efforts. Prior to preparing the Saticoy & Wells Community Plan, an extensive public process and thorough data collection phase was completed. The City sponsored a variety of well-attended public workshops over several months to gather and incorporate meaningful public input. Students from the City and Regional Planning Department at California Polytechnic State University, San Luis Obispo, assisted City staff with many of these workshops. These workshops are



Figure 11-4 - Community Plan Area

summarized below.

**December 3, 2005** - The community received a presentation summarizing important characteristics of the Plan Area. After the presentation, the audience broke out into small groups to discuss preferred attributes, concerns, and suggestions for improvements in the area. A survey was also distributed to collect additional information on matters of community importance, and destinations to which residents travel for services, schools, and shopping.

**January 19, 2006** - At this workshop, information collected in December was presented and a visual preference survey was conducted. The results of the table discussions held in December were summarized and a composite map with key community input was presented. The results of the December survey were also tabulated and presented. The visual preference survey contained over sixty slides with images representing a variety of intensities, streetscapes, building types, mixed-use development, park configurations, and options for Old Town Saticoy. In general, the 75 respondents to the visual preference survey liked images that evoked a sense of place and small-town character. However, respondents also overwhelming favored mixed-use development when presented at three to four stories or less.

**February 11, 2006** - The purpose of this workshop was to provide an overview of work completed, present the results of the visual preference survey conducted in January, introduce a number of ideas for the future, and gather additional community input. To introduce ideas for the future, the Cal Poly students developed scenarios for development on several of the large undeveloped sites and for Old Town Saticoy. These concept plans focused on connecting undeveloped areas with the existing built environment, creating walkable neighborhoods, providing more parks and

public open space, and establishing mixed-use neighborhood centers. The plans were presented to the entire audience and then discussed in smaller groups. Cal Poly prepared a preferred concept plan based on these discussions, which was presented by the students to the City Council on February 27, 2006.

The City retained the services of two technical consulting firms. One firm was hired to oversee the work prepared by the university and to lead the preparation of the Community Plan. The other firm was hired for urban design expertise and to work with the landowners, developers, and architects of proposed infill projects to ensure that City goals and community concerns were addressed in each proposal. These meetings, collectively known as the “Saticoy Sessions”, were held with the purpose of creating a cohesive master plan and a vibrant public realm for Saticoy and Wells. This work is summarized in the *Saticoy and Wells Design Plan* (October 2006). The Design Plan outlines an integrated street network, describes the form and character of six proposed neighborhoods, establishes a park and open space system, and provides a strategy for transit.

The Saticoy & Wells Community Plan is the product of General Plan implementation through public participation and the Saticoy & Wells Design Plan.

**Saticoy & Wells Planning Principles**

The intent of the Saticoy & Wells Community Plan is to weave existing neighborhoods into a more integrated community. This is primarily achieved by extending bike and pedestrian trails and creating street linkages where circulation systems are presently disjointed.

1. **Traditional Neighborhood Development (TND)** is consistent with Chapter 3 in the General Plan. TND is a method of planning wherein individual blocks and a continuous network of narrow streets encourage pedestrian movement.
2. **Make great public places** - The focus of a community's public life is in the richness of its open spaces and civic buildings. Connecting the Santa Clara River and the barrancas with a variety of new and varied parks, as well as shops and workplaces, has the potential to add unique social and economic assets to the Saticoy & Wells Community Plan Area. Currently, the Fritz Huntsinger Youth Sports Complex, the Saticoy Regional Golf Course (open to the public for a fee), and Saticoy Neighborhood Park in Old Town are the only park spaces in Saticoy and Wells.
3. **Generate a continuous network of great thoroughfares** - A properly designed interconnected network of regional and local thoroughfares can provide multiple routes to diffuse traffic, keep local traffic off of regional roads, and keep through traffic off of local streets while also encouraging pedestrian and bicycle movement. In order to increase safety, on-street parking should buffer the pedestrian from moving traffic,

where possible. Additionally, a diffused traffic network increases the options emergency personnel have to reach a distressed location.

4. **Make great neighborhoods** - The promise of future infill development brings the ability of new projects to create cohesive neighborhoods. The typical size of a walkable neighborhood is a quarter-mile (five minute walk) from center to edge. Well-designed neighborhoods are structured on a pedestrian-friendly network of blocks and thoroughfares that slows cars while encouraging pedestrian activity. Various open spaces and community gathering places are interconnected to facilitate a harmonious public life.
5. **Create a variety of housing choices** - Variety in housing choices boosts the potential to meet the needs of an increasingly diverse population. Moreover, mixing housing types, such as courtyard housing, row housing, even duplexes, triplexes and quadplexes, flats above commercial ground floor, with single-family units can create a rich neighborhood fabric. The intention is to not only foster a diverse community, but also to accommodate a range of income levels by design.
6. **Live near transit** - The presence of the historic train depot, which could become a commuter station in the future has the potential to further enhance the quality of life. The opportunity to live near transit appeals to people who like additional choices for travel and the vitality that surrounds transit-oriented districts. As such, the area around the depot in Old Town Saticoy should allow more density over the long-term.

A community plan is adopted by resolution as an amendment to the General Plan under Government Code Sec. 65350, et seq. It refines the policies of the General Plan as they apply to a smaller geographic area and provides additional policy direction at the neighborhood level. Community plans are implemented by ordinances and other discretionary actions, such as zoning and development codes (*General Plan Guidelines, Governor's Office of Planning and Research, 2003*).



Community Workshop

7. **Get the retail right** - The retail industry’s shift from closed malls to outdoor “Main Street” type retail is a response to the changing lifestyles of customers, often yielding higher sales volumes than shopping centers. Retail should be concentrated along streets that accommodate pedestrian shop-front stores and provide on-street parking.

This Community Plan requires community gathering spaces within each of the six distinct and interconnected neighborhoods upon which this Plan is based. These community gathering spaces are intended to be different from one neighborhood to the next with various focal points and intensities. For instance, some neighborhoods will have varying degrees of retail, while others may simply have some offices or a community facility. The East Neighborhood is envisioned to include the highest quantity of retail intended to serve the East Ventura Communities. Old Town Saticoy, within the Southeast Neighborhood, presents a unique characteristic emanating from its historic role as a traditional town center, complete with civic functions. Old Town Saticoy also is the home for a potential commuter rail stop. Each community gathering space, or focal point, within each neighborhood should be designed to meet neighborhood demands and to complement one another rather than to compete and dilute the retail potential.

8. **Encourage various modes of transit** - Provision for various modes of transit from buses and cars, to bikes and pedestrians, lies at the heart of great place-making. This Plan envisions the calibration of streets and

thoroughfares to encourage and enhance a wide set of transit possibilities, with the train depot acting as a potential future transit mode.

9. **Get the parking right** - The compact, mixed-use nature and walkability of a neighborhood lends itself to a park-once strategy where it is possible to park once and walk to a number of destinations. By completing multiple tasks on foot, this transforms drivers into pedestrians and is the secret of great street vitality. This plan anticipates modified parking standards in appropriate locations to help achieve this intention.

10. **Maintain industry functions** – One of the City’s largest contiguous areas of industrial lands is concentrated in the Saticoy, or in the Southeast Neighborhood as identified in this Plan. To remain a viable industrial base for the City, this Community Plan preserves the existing Industry Planning Designations in order to continue to accommodate industrial uses. In addition, the City’s economic development strategies focus on the expansion and retention of industrial businesses, diversifying the local economy, and providing areas for increased job opportunities.”

11. **Manage natural resources through ‘infill first’ and green redevelopment** - The “Infill First” strategy of the 2005 Ventura General Plan is in and of itself a powerful environmental strategy to reduce the pressure to develop in greenfields and expand the urban growth boundary. Compact, walkable, transit-oriented community design minimizes

automobile-generated urban runoff pollutants and the open lands that absorb water are preserved to the maximum extent possible. Overall the collective urban design principles contained in this plan work to reduce the footprint and impacts of development by efficiently using land, sharing public spaces such as parking and transportation, and reducing, per unit, the amount of impervious cover and land disturbance needed to sustain our community and development needs. This plan will introduce an array of green features to minimize runoff, prevent pollution, reduce water use and auto travel-related pollution. Actions at the areawide scale will focus on identifying and using existing natural drainage systems to promote water treatment through infiltration to the extent feasible. Areawide solutions, coupled with site level practices, can be designed to minimize and prevent impacts on water quality and reduce stormwater quantity.

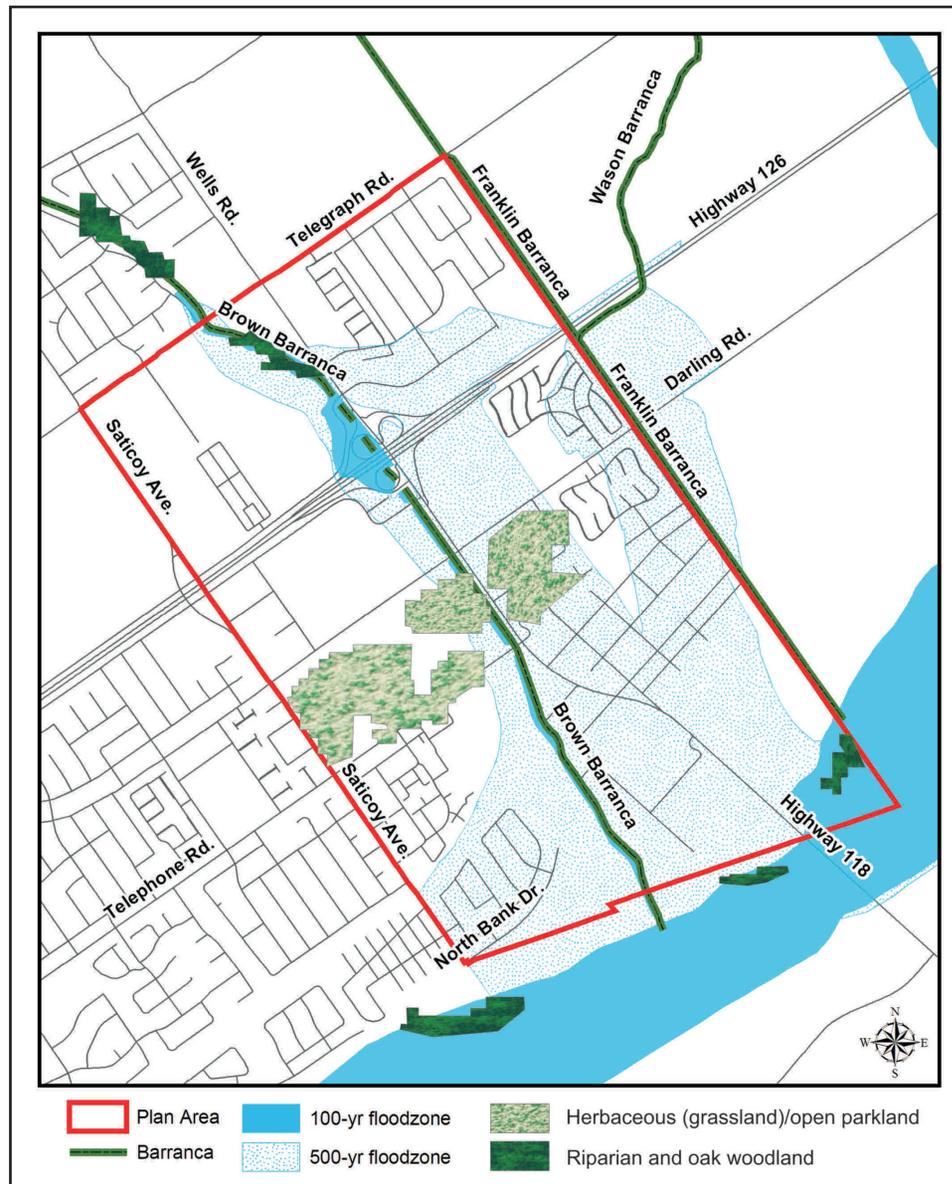


Figure 11-5 - Floodzone and Natural Resources

### 11.1 Our Natural Community

The Saticoy & Wells Community Plan Area contains remnants of the natural environment that existed in the area before large-scale agriculture and urban development began to alter the landscape. Some of the remaining natural features include the Santa Clara River floodplain and sections of the Brown and Franklin Barrancas (see Figure 11-5).

The health of the community is inextricably linked to the health of the natural environment. This section addresses preservation of the existing natural habitats and restoration of those areas that have been completely altered by human impacts. Furthermore, this section focuses on the beneficial values these natural areas provide for the community, such as contributing to larger ecosystems and providing buffers to ease conflicts between competing uses.

**Policy 11A: Restore and maintain critical environmental habitats, such as the Brown and Franklin Barrancas and the Santa Clara River, as vital components of the natural resource system for wildlife habitat, water quality through sub-basin stormwater collection and for recreation opportunities.**

Action 11.1.1: Where land or structural improvements are necessary to the barrancas or river, development should comply with the Ventura County Watershed Protection District standards and permit requirements, and require the incorporation of aesthetic and ecologically sensitive design treatments.

Action 11.1.2: To the extent possible, preserve the Brown and Franklin Barrancas and the Santa Clara River in their natural state.

Action 11.1.3: Enhance the Brown Barranca along the western edge of Wells Road to create a distinct green edge. Any development that happens along the golf course in the long term will have to further improve this green edge.

Action 11.1.4: Work with the County of Ventura and the Watershed Protection District and other appropriate agencies to convert the concrete channel of the Franklin Barranca to a natural watercourse.

Action 11.1.5: Create a linear park along the Brown Barranca and ensure its visual and experiential continuity from north to south over the freeway.

Action 11.1.6: Require landscape that conserves and re-establishes native habitat in the riparian corridors, protects drainage processes, reduces water demand, retains runoff, and recharges groundwater supplies.

Action 11.1.7: Work with local watershed groups and others to identify opportunities to protect natural features and systems including trees and vegetation, soils, hydrology, and to restore features such as urban creeks and wetlands that have been degraded from previous land uses and management practice.



**Brown Barranca**



**Franklin Barranca**

## **11.2 Our Prosperous Community**

Positioned around two major intersecting arterials at Highway 126 and Highway 118 (Wells Road), Saticoy and Wells is located in the east end of the city with opportunity for community serving retail, business, and offices to serve this segment of the population while protecting the environmental qualities that are so important to residents. Moreover, a generous industrial section in proximity to SOAR- protected farmlands (Save Our Agricultural Resources), rail lines, and Wells Road suits the area to agricultural support services, such as biotechnology and other production, distribution, and repair industries.

The Plan Area is proposed to be developed through a series of six integrated neighborhoods, mixed-use thoroughfares, and connected natural resource areas (see Section 11.3). These neighborhood linkages can promote retail and service industries and create shopping and business service opportunities for the east side of Ventura.

A variety of retail, office, shopping, and dining opportunities are called for throughout the Plan Area. As mentioned previously under the Saticoy & Wells Planning Principles (pg. 11-12 & 11-13), getting the retail right while preserving the industrial base is a centerpiece of this Plan. As discussed, a variety of opportunities should exist in the Plan Area that offer different intensities of neighborhood and workplace serving retail. This Plan intends that Old Town Saticoy remain the historic Town Center of the entire Plan Area, complete with civic functions, residential and office space, and an anticipated Transit Oriented Development (TOD). The existing Saticoy Village Specific Plan area, combined with new development just to the north of Darling Road is intended to be developed as a village center, with retail opportunities to serve the larger community.

The remaining neighborhoods in the Plan Area are intended to provide lesser intense retail or office space, or community facilities, to serve the immediate neighborhood. In addition, within the Plan Area is one of the City's larger contiguous industrial sectors that is intended to remain intact, and unchanged, with the exception for some work-live and workplace serving opportunities. Together, these retail, office, and industrial uses will provide for a prosperous economic development over the long term.

The City of Ventura recognizes that prosperity is to be shared with all segments of the community and that services and jobs for struggling families, the disadvantaged, or the disabled is part of what makes a community integrated, healthy, and safe. This Plan aspires to protect the economic interests of all residents by encouraging diversity in employment opportunities and social service delivery.

### **Policy 11B: Maintain and improve Old Town Saticoy's historic town center identity.**

Action 11.2.1: Form a local economic development advisory body with staff support to work with the City and County of Ventura to:

- Solicit interest from property owners to participate in revitalization activities,
- Promote private/public partnerships that target investments and stimulate economic growth, and
- Create incentives to upgrade and renovate properties.

**Policy 11C: Support economic growth and the creation of a wide-range of jobs through business retention, expansion, and formation in the Saticoy Industrial District, and throughout the six neighborhoods.**

Action 11.2.2: Intensify land uses in the Saticoy Industrial District to capitalize on the transit and agricultural location in collaboration with the Ventura County Resource Management Agency.

Action 11.2.3: Formulate an economic development strategy for the Plan Area that fosters business retention and expansion by identifying the appropriate scale and diversification of commercial and industrial activity, such as biotechnology and other agricultural support businesses.

Action 11.2.4: Concurrent with annexation, survey the Saticoy Industrial District for possible blight conditions, reconfiguration of land parcels, and under utilization of prime areas to capture industrial land use value.

Action 11.2.5: Develop urban standards for the East Neighborhood, in particular the Saticoy Village Specific Plan area and along the frontages of Darling Road, to allow development of community serving retail.

Action 11.2.6: Develop urban standards for the Saticoy Village Specific plan site, and the Broome site (APN 090-0-081-235), that require a minimum of 50,000 square feet of commercial uses on each site provided that the predominant use of the Broome site remain residential.

Action 11.2.7: Design commercial development in the East Neighborhood with buildings that front the streets, that incorporate a mix of uses, that are

situated within a proscribed block structure, and that provide parking in an urban format.

**Policy 11D: Maintain the Saticoy Industrial District and all other Industry Planning Designations as a primary industrial sector for the east end of Ventura.**

Action 11.2.8: Prioritize and phase capital facilities infrastructure in the Saticoy Industrial District by working with the Ventura County Public Works and Resource Management Agencies.

Action 11.2.9: Develop standards that protect industrial sites from incompatible uses.

### **11.3 Our Well Planned and Designed Community**

Many residents in the Plan Area want to enhance the quality of life within their neighborhoods. Therefore, new development should be of the highest design quality. It should also provide much needed public amenities, such as parks and community facilities, and meet larger citywide goals. These citywide goals include creating walkable, compact neighborhoods with a wide diversity of housing types and neighborhood serving uses in order to reduce absolute reliance on the automobile and to increase housing choices. Greater intensity and mixed-uses are called for along Los Angeles Avenue and along Wells Road. This Plan attempts to balance the public purpose of the citywide initiatives with existing neighborhoods.

#### **Neighborhoods**

In contrast to the historic Old Town Saticoy block and street grid immediately adjacent to the rail tracks, much of the post war development in the area displays the typical pattern of sprawl development. These housing tracts are made up of a discontinuous street pattern wherein internal streets often end in cul-de-sacs and only one or two connections link the developments to the surrounding arterials. The block lengths are as long as 1,000 feet in some cases, with wide streets fronting them. The housing fabric is homogeneous and is the result of repetition of one or two housing types. The communal open spaces in these tracts, where present, are completely introverted and disconnected of their surroundings.

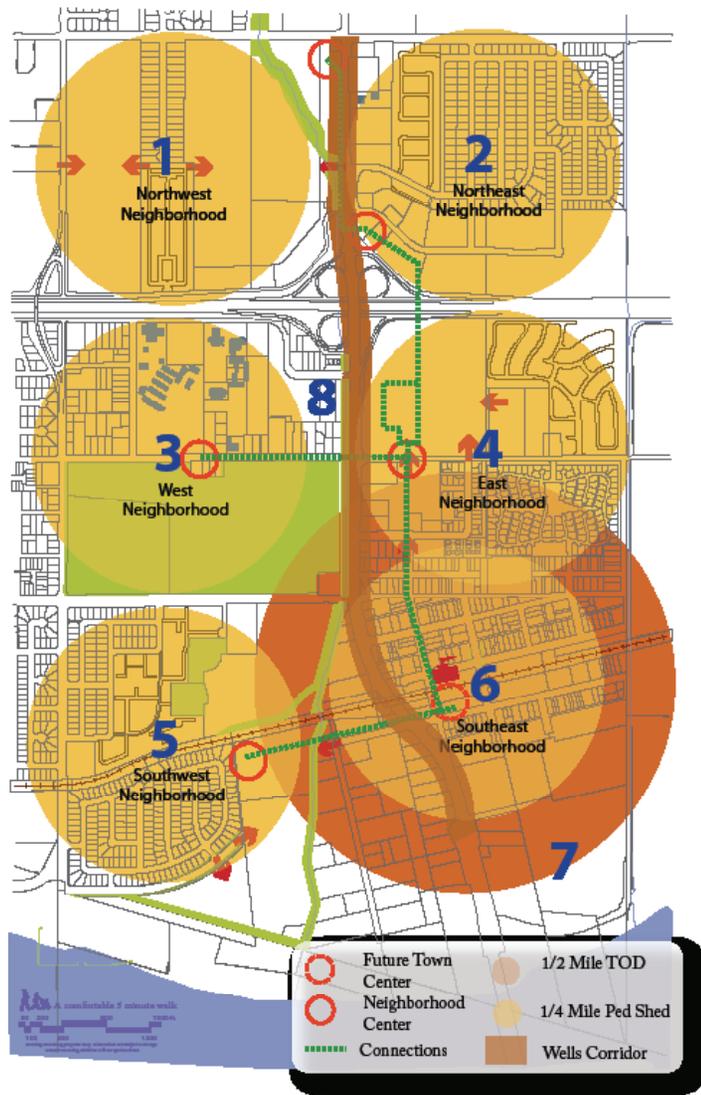
What is observed is a pattern of development where each tract is designed in autonomy, with little cohesive structure and little or no relationship to its larger

surroundings. The promise of future development in this area lies in its ability to create newer urban patterns that can both restructure the fragmented fabric, absorb these autonomous tracts, while serving as a positive catalyst for future development.

One of the challenges of this Plan is to coalesce new infill development into an integrated framework, while leaving enough flexibility for detailed urban design components. The policies and actions in this section provide direction for new development to achieve the larger vision of the Saticoy & Wells Community Plan.

The Saticoy & Wells Plan Area is envisioned as six interconnected neighborhoods, each with its own distinct community gathering place and community amenities (see Figure 11-6). This structure better facilitates the relationship of new infill development to the existing housing tracts and their need for communal facilities. The various neighborhoods are each separated in some way by Highway 126 and/or Wells Road, which act as significant neighborhood edges. The architectural character of each neighborhood face to Wells Road becomes a significant component of the Plan.

- 1. Northwest Neighborhood** - This neighborhood is framed by Telegraph Road, Saticoy Avenue, Wells Road and Highway 126. It currently consists of an existing housing tract and a mobile home park. Two Specific Plans, totaling over 100 acres, were recently approved for this area. Various parks and open space types, and community gathering places are envisioned, including the potential for future corner retail along the intersection of Wells and Telegraph Roads. An interesting feature of this neighborhood is the barranca, which should be enhanced as a passive neighborhood amenity.



2. **Northeast Neighborhood** - This neighborhood is framed by Telegraph Road, Wells Road, Highway 126, and the SOAR fields to the east. It is largely made up of post-war, irregularly developed housing tracts with few connections to the surrounding arterials, and no communal open spaces. New development on the parcel(s) at Citrus Place and Peach Avenue (22 acres) in the southwest corner will provide a much needed park. The under-used parcels along Wells Road and parcels near the Highway 126 interchange present great opportunities for revitalized neighborhood serving commercial development.
  
3. **West Neighborhood** - This area is primarily a combination of school facilities and under utilized parcels along Saticoy Avenue. With the current lease of the County-owned Saticoy Regional Golf Course expiring in 2016, it could become an important public use with community amenities for the entire area. As such, it needs to be considered as part of the Plan vision. If converted, the site has the potential to create a setting for a public school, significantly add to the open space network, and enhance the linear park along the Brown Barranca.
  
4. **East Neighborhood** - This neighborhood is framed by Highway 126 to the north, Wells Road to the west, and the agricultural fields to the east. While it is generally comprised of disconnected housing tracts, this neighborhood has a great opportunity for enhancement. Potential infill development on nearly 60 acres represents an opportunity to develop community-serving retail along

Figure 11-6 - Neighborhoods within the Community Plan Area (numbers correspond to Neighborhood Descriptions in the text, while 7 and 8 refer to the TOD and Wells Road Corridor respectively).



**Mural in Old Town Saticoy**



**Illustration of Retail**

with housing, and provide additional public amenities.

5. **Southwest Neighborhood** - The south-west neighborhood is bisected by the rail tracks and bounded on its southern edge by the Santa Clara River. As such, the feasibility of a pedestrian connection across the tracks should be investigated. This potential pedestrian connection as well as access and frontage towards the river become important aspects of neighborhood design. One of its most sensitive areas includes the historic Chumash Indian burial grounds which can become a significant civic and open space amenity for the neighborhood. This neighborhood is also home to a new Veteran’s Home located just south of Telephone Road. With North Bank Drive separating the walled housing tracts to its north from their surroundings, it becomes important to create a pedestrian-friendly block and street structure along with a new center to enhance the potential of this neighborhood shed.

6. **Southeast Neighborhood** - The south-eastern neighborhood centers around historic Old Town Saticoy, which is located immediately north and south of the existing rail tracks. Several historic sites can be found here, including the Farmers & Merchants Bank, Walnut Growers Association Warehouse, and the Saticoy Bean Warehouse.

The existing development pattern of this area reveals its historic urbanism through a network of rectilinear streets that run north-south and east-west, generating a clear pattern of blocks that are typically 400 feet long and 300 feet wide with alleys delineating

fronts and backs. This network of streets is made up of sporadic and discontinuous retail and commercial uses, and gradually dissolves into a disconnected block-street network with several dead-ends. However, the existing street network provides the opportunity to redevelop the grid pattern into a more urban environment.

Under this Plan, Old Town Saticoy is recognized as the historic town center for the area and as such, is intended to serve as the primary location for civic uses with office and retail uses for the entire Saticoy & Wells Plan Area and for eastern Ventura, allowing the communities to benefit from the concentration of existing commercial and industrial uses. In addition, proximity to Highway 126, Highway 118 (Wells Road), and the rail system provide excellent regional connectivity. Tremendous opportunity exists for new development and employment in the Saticoy Industrial District.

7. **Transit Oriented Development** - The historic train depot and rail tracks present the potential for a commuter train and encourages the idea of a Transit Oriented District in the mid to long term. The presence of a train may attract commuters from up to a half mile from the transit center. The larger pedestrian shed overlaps with some of the surrounding neighborhoods and significant portions of the remaining undeveloped land in the Plan Area. (See “7” on Figure 11-6)
8. **Wells Road Corridor**- Wells Road separates the neighborhoods described above. However, there is potential to reconfigure Wells Road to allow increased connectivity from east to west. Under this Plan, Wells Road is delineated as

a Corridor from Telegraph Road to Nardo Street. Buildings along Wells Road will be required to address the street and should be of appropriate mass and scale to create a visual impact for the heavily trafficked thoroughfare. New development along Wells Road will also enhance the surrounding neighborhoods and buffer them from intruding noise and light pollution. The goal is to activate the street level and create a rich interface between Wells Road and the adjoining neighborhoods (see “8” on Figure 11-6). (See also Wells Road Corridor in Section 11.4.)

**Historic Character**

The historic town of Saticoy was laid out in the late 1800’s on lands that previously were inhabited by the Chumash civilization. The character of Saticoy, now known as Old Town Saticoy, is largely attributable to its one and two storey housing. Small bungalow style, single-family houses, some of which are now in need of repair, line Violeta, Azahar, and Nardo Streets. A handful of small bungalow-style houses facing the south side of the rail tracks are older than the majority of the homes on the north side of the tracks. In some areas, Quonset huts that have been converted into homes are unique features in this neighborhood.

Historic buildings in Old Town Saticoy, all with farming-related uses, include the Saticoy Walnut Grower’s Association Warehouse on Wells Road, the Saticoy Warehouse Company (Bean Warehouse) on Azahar Street, and the Farmer’s and Merchant’s Bank of Santa Paula (Saticoy Branch) on Los Angeles Avenue. Many of them, though not in the best condition, represent a significant opportunity to preserve the history of this area.

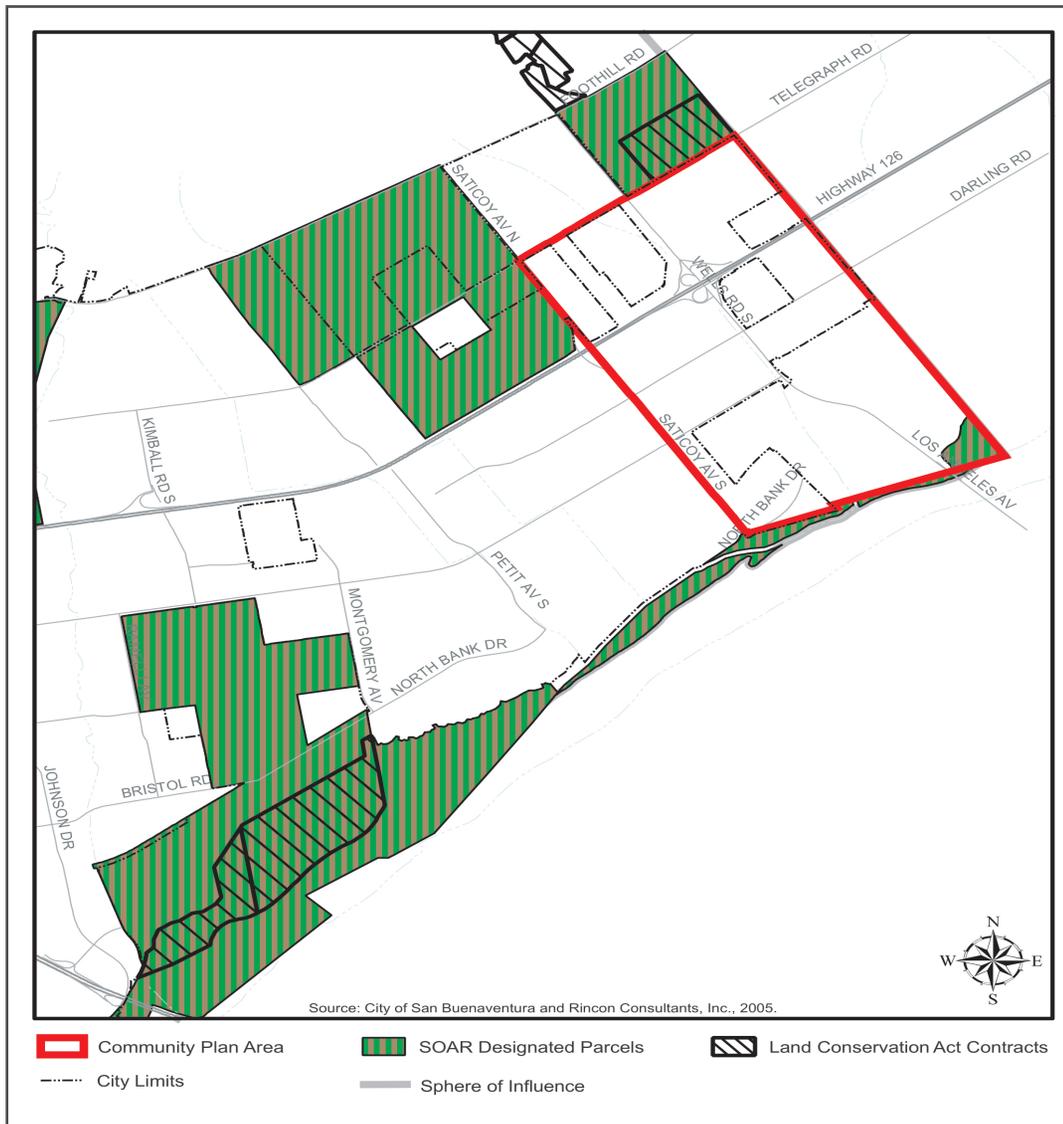
**Infill Plans**

Infill projects should ensure traditional neighborhood development with community gathering places, park and open space area, opportunities for civic facilities, and additional workforce housing. Neighborhood infill projects are envisioned with the following constituent elements:

- Easy pedestrian access to urban and natural amenities,
- A five-minute pedestrian walking shed from center to edge,
- An interconnected north-south and east-west network of diverse pedestrian-friendly thoroughfares,
- A rich and interconnected palette of open spaces, from streets, to parks, to playing fields, trails, and watercourses,
- Functional and aesthetic green design and infrastructure practices to help reduce water demand, recharge groundwater, treat and retain runoff, and decrease flood risks,
- A set of civic, educational, and community facilities that serve public life, and
- A variety of housing types for people of a variety of incomes and ages.

**Special Housing Needs**

There are several mobile home parks in the Saticoy & Wells Community Plan Area. While not designated as such, mobile home parks are considered to be a form of low-cost housing, particularly suitable to the needs of senior citizens. Consistent with the City’s Housing Element, it is the intention of this Plan to provide for the long-term protection of the mobile home parks.



**Figure 11-7 - SOAR Properties**

According to the General Plan Background Report (2002), the homeless form encampments in the Santa Clara riverbed at the southern border of the Community Plan Area. This Plan recognizes this issue and proposes that the City work with the County of Ventura and the Continuum of Care Consortium Homeless Task Force to determine the need for homeless facilities in the Plan Area.

The Ventura County Farmworker Housing Study (2002) identified 24 parcels in and around the Saticoy & Wells Community Plan Area that are potentially suitable for development of farmworker housing complexes. As parcels are annexed into the City, this plan proposes that City work with the County to secure funding and identify appropriate locations for farmworker housing.

**Agricultural Lands**

The soil in parts of the Community Plan Area are classified as prime agricultural land, which is the most productive land in the state. Approximately 160 acres of land designated for development is currently or has been used for the cultivation of nursery crops, seeds, truck crops, and lemons. There are no parcels in the Plan Area subject to either the Williamson Act or the City SOAR initiative (see Figure 11-7).

The eastern, northern, and part of the western boundaries of Plan Area adjoin large productive agriculture areas, which will remain as viable agriculture. The need to buffer residential and community uses from agricultural activity is addressed in this Plan.

**Policy 11E: Sustain and complement the historic and natural characteristics of the Saticoy & Wells Community Plan Area.**

Action 11.3.1: Develop Old Town Saticoy, the historic core of Saticoy & Wells, through lot-by-lot infill that respects the character of the existing urban fabric.

Action 11.3.2: Ensure the frontage of Wells Road, south of Darling Road, enhances the historic character of Old Town Saticoy.

Action 11.3.3: Provide the southern gateway to Saticoy with public art on the triangular parcel of land at the intersection of Nardo Street, Los Angeles Avenue, and Wells Road. This feature could be added to development on the site.

Action 11.3.4: Work with the Historic Preservation Committee to preserve important historic buildings in the area through reuse and preservation.

Action 11.3.5: Seek joint adoption of the Saticoy & Wells Community Plan and accompanying development code with the County of Ventura until the City annexes unincorporated areas.

Action 11.3.6: Work with property owners in the unincorporated areas of Saticoy to encourage annexation of Old Town Saticoy and the Saticoy Industrial District.

Action 11.3.7: Monitor the production and pace of new development through the issuance of building permits and report annually to the City Council. When the Plan Area has reached 70% of predicted development as defined in the General Plan, the City Council shall review the intensity of development and locations throughout the Plan Area to determine if strategies are needed to modify the pace, redirect

location of new growth, or consider changing the Planning Designations and related zoning for the area.

**Policy 11F: Integrate the design principles of Traditional Neighborhood Development into community-scale and building-scale plans.**

Action 11.3.8: Design the Saticoy & Wells Community Plan Area as a series of six neighborhoods with community gathering places within a network of interconnected blocks.

Action 11.3.9: Ensure infill is integrated with surrounding development to achieve continuity of design and scale and connectivity of open space and circulation patterns.

Action 11.3.10: Work with Caltrans to reconfigure Wells Road with new buildings and uses to establish it as a pedestrian friendly, mixed-use thoroughfare.

Action 11.3.11: Create development standards that allow existing neighborhoods to change over time to reflect the community design features of this Community Plan.

Action 11.3.12: Allow and encourage small retail and restaurant areas within walking distance of the industrial employment centers.

Action 11.3.13: Create urban standards for parcels along Wells Road to both create an urban face to the proposed Wells Road Corridor, as well as transition down into neighborhood massing and densities. Projects along Wells Road should have urban frontages such shop fronts, and live work housing types.

Action 11.3.14: Establish a live/work flex frontage for the units just west of the Brown Barranca in the Southwest Neighborhood in anticipation of their relationship and context facing the Saticoy Industrial area.

**Policy 11G: Promote the development of neighborhood centers at strategic locations to direct investment into the local economy, encourage community vitality, and provide community amenities.**

Action 11.3.15: Enhance the intersection at Wells and Telegraph Roads as a new urban node with parallel parking along the street and higher density mixed-use structures at the corners.

Action 11.3.16: At the intersection at Wells and Darling Roads require community serving retail with appropriate building types that extends east along Darling Road to just beyond Los Angeles Avenue. Require similar frontages along the edges of this segment of Darling Road.<sup>1</sup>

Action 11.3.17: Require a new community gathering place in the Southwest Neighborhood along the extension of North Bank Road to serve the local neighborhood as well as the workforce in the Saticoy Industrial District.

Action 11.3.18: Encourage concentrated retail and commercial infill along Los Angeles Avenue between Nardo Street and one block north of Violeta Streets.

Action 11.3.19: Intensify land uses around the historic train depot in Old Town Saticoy to allow it to develop as a commuter transit center.

**Policy 11H: Diversify housing to provide for a range of incomes and special needs throughout the Saticoy & Wells Community Plan Area.**

Action 11.3.20: Design each of the six neighborhoods to include a wide range of housing types, from single-family detached to row houses and apartments.

Action 11.3.21: Consistent with the City of Ventura Housing Element, provide for long-term mobile home park protection.

Action 11.3.22: Analyze sites for farmworker housing using the County of Ventura *Farmworker Housing Study* (2002) and identify potential alternative sites as new development is proposed.

Action 11.3.23: Consistent with the City of Ventura Housing Element, provide financial and regulatory incentives for workforce, live-work, and work-live housing in mixed-use and industrial areas.

Action 11.3.24: Coordinate efforts with the County of Ventura and the Continuum of Care Consortium Homeless Task Force to determine the need for homeless facilities in the Plan Area.

**Policy 11I: Continue to preserve agricultural uses in the City's Sphere of Influence and as identified in the greenbelt agreement between the City of Ventura and Santa Paula, and require new development to provide all necessary buffers.**

Action 11.3.25: Collaborate with the County Agriculture Commissioner's Office to determine the necessity for agricultural buffers in new development. If required, agricultural buffers shall be no less than 300 feet to new structures and sensitive uses on non-agricultural property, but may be reduced to no less than 150 feet on recommendation or guideline of the agricultural commissioner for vegetative screens or other buffering mechanisms to protect neighborhoods from agricultural activities and to allow agricultural uses to continue operating.

Action 11.3.26: Utilize CEQA to identify mitigation

Adopted November 2009

measures such as agricultural buffers to be employed by new development to reduce impacts as determined by applicable thresholds of significance for noise, toxic substances, odors, and other effects of agricultural use as it adjoins the boundaries of new development within the Saticoy and Wells Community Plan Area.

**Policy 11J: Incorporate green design and infrastructure solutions into the urban landscape using low impact development techniques to protect and preserve water resources, and mitigate air quality and urban heat island effects.**

Action 11.3.27: Require new development to utilize low impact and green design techniques to treat stormwater and mitigate air quality and urban heat island effects.

Action 11.3.28: Require new development to minimize impervious surfaces through compact design, parking reduction strategies, street design, and the use of alternative paving surfaces where applicable.

Action 11.3.29: Require landscaping to reduce water demand, retain runoff, decrease flooding, and recharge groundwater through selection of plants, soil preparation, and the installation of appropriate irrigation systems.



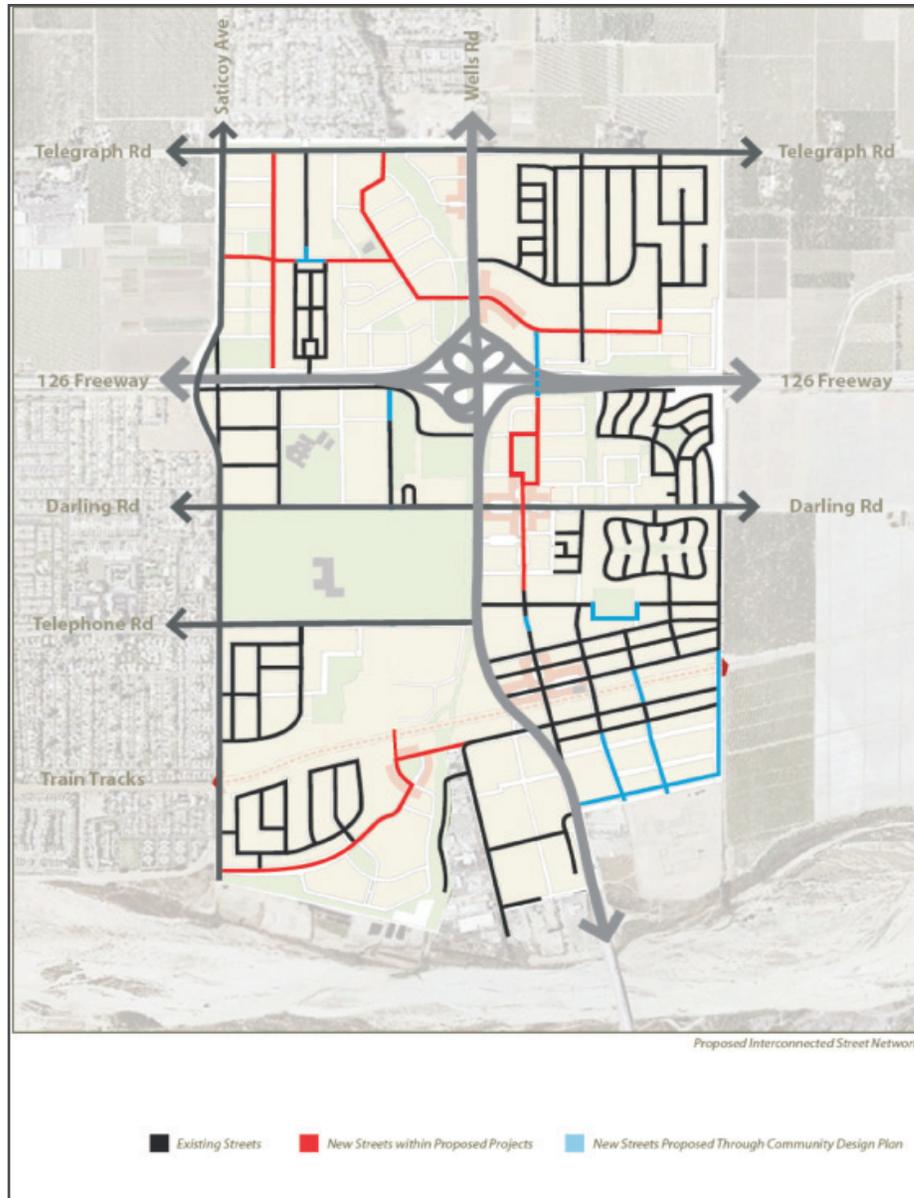


Figure 11-8 - Proposed Street Network

Adopted November 2009

### 11.4 Our Accessible Community

When appropriate attention is paid to the design elements of streets, the results can transform the aesthetics of a region or a neighborhood and create distinct community character. This occurs when the design elements of transportation conform to the design elements of land use and neighborhood planning. Working together, well thought out communities can form the distinct “sense of place” upon which this Plan is based.

The present transportation network in the Saticoy & Wells Community Plan Area facilitates rapid movement of cars, making many of the roads unsafe for pedestrians. Moreover, transit, pedestrian, and bicycle access are limited by infrequent transit service, poor infrastructure, and sprawl land patterns. While neighborhood streets within Old Town Saticoy follow a formal grid pattern, newer streets are winding and sporadic, with minimal connections to main arterials and other neighborhoods.

There is an overall lack of interconnectivity amongst streets and thoroughfares, thereby relying on the main arterials to carry most of the traffic volume. Weaving the fragmented thoroughfares together is one of the most important tasks of this Community Plan.

The fundamental strategy in revitalizing the Saticoy & Wells Plan Area is the transformation of its existing fragmented and disconnected street pattern into an integrated local network of north-south and east-west thoroughfares of various types. An integrated thoroughfare network will help disperse traffic evenly without pressuring the major arterials to carry larger traffic volumes and allow safe movement of pedestrians, which is a key component of walkable neighborhoods.



**Illustration of Thoroughfares**

**Interconnected Thoroughfares**

There are two primary ways in which the plan intends to weave the internal street network into an interconnected network (see Figure 11-8). First, streets in the proposed infill projects of the Saticoy and Wells Plan Area present a distinct advantage, in that, if properly designed they can serve to bring together the existing disconnected street network. Thus, specific projects introduce new streets that establish connections from north to south and east to west. Second, area-wide street interventions (shown in blue in the adjoining diagram) help further weave the street grid into a cohesive network.

The principle strategies to establish an interconnected street network in the Saticoy and Wells Area are as follows:

1. A new north-south connection (Los Angeles Avenue), east of Wells Road will provide alternatives for local traffic that normally bottlenecks on Wells Road. Los Angeles Avenue is intended to connect to Darling Road and beyond.
2. A pedestrian crossing from the East Neighborhood to the Northeast Neighborhood will be evaluated in order to link these two neighborhoods together, allowing increased access to neighborhoods to the south.
3. The enhancement of Telegraph Road will establish a clear east-west connection north of the Freeway.<sup>1</sup>
4. The enhancement of Darling Road will establish a clear east-west connection south of the Freeway.

5. Two east-west connector roads at Citrus Drive (north of the 126) and Nardo Street (south of the train tracks), will join the area latitudinally and provide alternatives to the 126, Darling Road, and Telegraph Road for local traffic.
6. In new development, local streets will ensure interconnectivity between different projects within the same neighborhood, through compliance with subdivisions standards of this Plan's accompanying development code, and further connect to other neighborhoods.
7. Explore the potential for new at-grade rail pedestrian crossing located west of Wells Road to complement the two existing crossings that exist to the east of Wells Road.
8. Enhancements for Wells Road, from Telegraph Road in the north to Nardo Street in the south, are intended to strengthen this thoroughfare's character to allow pedestrians, cars, bicyclists, and other modes of transit to coexist with a mix of uses to activate the street level and weaken its current presence as a distinct barrier between neighborhoods.<sup>1</sup>
9. Old Town Saticoy will maintain its grid pattern and further improve upon it through the extension and connection of stubbed streets to provide for a greater degree of connectivity.

### Thoroughfare Types

Various streetscapes will define the different characters of the thoroughfares, establishing a clear hierarchy of parkways, avenues, and local streets. Detailed descriptions of streets, including their hierarchy and intended design attributes are located in the accompanying Saticoy & Wells Code.

### Wells Road Corridor

Wells Road is the spine of the six neighborhoods described in Our Well Planned and Designed Section. This Plan attempts to strengthen the road's character as a distinct corridor where pedestrians, cars, and other modes of transit can coexist along with a mix of uses, thereby activating the street level and creating a rich interface with the adjoining neighborhoods. The City of Ventura controls the right-of-way to Wells Road between Telegraph Road and Citrus Drive (the northern segment), and Caltrans owns the right-of-way between Citrus Road and the southern Plan boundary. Any reconfiguration plans for Wells Road must account for how these two independently-controlled portions of the thoroughfare may develop, given the different regulatory standards and jurisdictions. The Plan establishes two options for Wells Road in the area controlled by Caltrans.

### Old Town Saticoy Transit and Circulation Components

With potential regional connections, the existing freight train line and the historic train depot, currently owned by Ventura County Transportation Commission (VCTC), present an important commuter transit opportunity, and can markedly enhance Old Town Saticoy's historic town center (see Figure 11-9).

The existing freight line and the historic train depot present an important commuter transit opportunity for the future. Two phases of the Ventura-Santa Clarita Rail Corridor Study have been completed by VCTC, which look at the potential and feasibility of the reappropriation of this line for passenger service.

Old Town Saticoy is envisioned to develop through several planned phases: (1) the reconfiguration of Los Angeles Avenue into a narrower parallel-parking thoroughfare with mixed-use commercial and live-work frontages on the ground floor; (2) the renovation of the rail station as a commuter line, which will transform Old Town Saticoy into a transit center beyond its presence as a mixed-use hub; and (3) the reconfiguration of Old Town Saticoy around the rail station as a new "main street" with angled parking and varied shop-fronts.



Existing Conditions of Wells Road



Neighborhood Bus Shelters

#### Transit Village Plans

The Transit Village Development Planning Act of 1994 provides for local jurisdictions to adopt Transit Village Plans for mixed-use centers at rail transit stations based upon findings that persons who live within a quarter-mile radius of rail services utilize the transit system in far greater numbers than the general public. Currently, only a few rail transit stations in California have any concentration of housing proximate to the station. The State of California provides funding and permit process incentives for jurisdictions who adopt a Transit Village Plan. (California General Plan Guidelines 2003)



Figure 11-9 - Existing Rail Network

**Parking and Transportation Demand Management**

Managing parking supply and demand provides for an efficient use of this resource in order to maintain the desired qualities of walkable neighborhoods. As new development is constructed in the Plan Area, there may be opportunities to look at shared parking arrangements. In addition, as time goes by and neighborhoods mature and reach a point where additional parking management approaches may be necessary, implementation of residential parking benefit districts may be desired. The City is currently embarking upon a Citywide Mobility Plan that will develop a menu of parking management programs that are intended to be implemented throughout the City as needed. The Saticoy and Wells area will be evaluated to determine if these programs will help improve the overall quality of life.

**Policy 11K: Improve thoroughfare design and ensure that the circulation system is interconnected and usable by all modes of transportation.**

**Area Wide Actions**

Action 11.4.1: Require street continuity and interconnectivity between infill projects (including neighborhood focal points) and existing development and through new subdivision standards.

Action 11.4.2: Develop street standards that emphasize the safe and sufficient movement of vehicles, pedestrian safety, streetscapes, and compatibility with adjoining urban features and incorporate naturalistic 'green street' design elements into the streetscape to minimize impacts to the natural environment.

The Gold Coast Transit System and the Ventura Intercity Service Transit Authority (VISTA) are the two bus systems that service the area. Bus routes pass through the area along the 126 Highway, Wells Road, Darling Road, through Old Town Saticoy, and along Telegraph and Telephone Roads (see Figure 11-10).

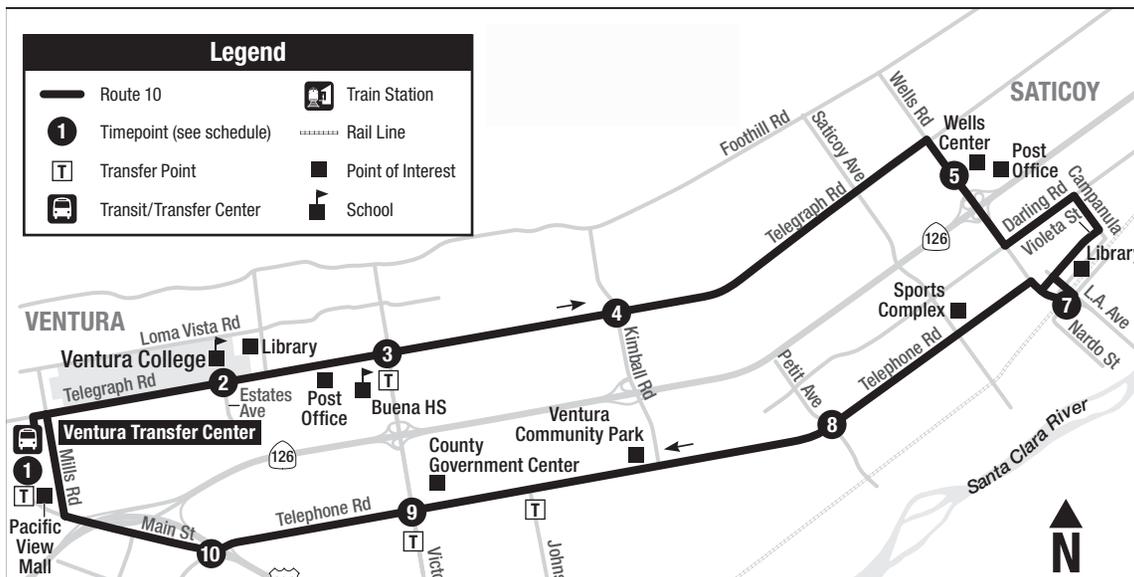


Figure 11-10 - Existing Gold Coast Transit Route

Action 11.4.3: Create standards for properties fronting Wells Road to facilitate the efficient movement of vehicles, bicyclists, and pedestrians between community gathering places, transportation nodes, and public areas, and to protect the public/private realm.

Action 11.4.4: Work with Caltrans to reconfigure Wells Road (south of Citrus Drive) to accommodate new buildings and uses and to create it as a pedestrian-friendly, mixed-use thoroughfare.

Action 11.4.5: Improve connectivity between schools and neighborhoods through pedestrian access across and along major thoroughfares.

Action 11.4.6: Develop a cohesive network of bicycle paths that link neighborhoods, community gathering places, and recreational areas as an extension of the City's "Select System of Bikeways."

Action 11.4.7: Require development projects adjacent to urban focal points and natural features to create seamless connections and easy pedestrian access to those features.

Action 11.4.8: Require, through new development or redevelopment, the installation of sidewalks.

Action 11.4.9: Pursue the relinquishment of Wells Road controlled by Caltrans.

Action 11.4.10: As needed, implement parking management strategies throughout the Community Plan Area.

Action 11.4.11: Revise the requirements of parking standards to allow for shared parking in appropriate locations as part of the accompanying development code for the Saticoy & Wells Plan Area.

Action 11.4.12: When it's appropriate to implement a Parking Benefit District, pursue a "Park-Once" strategy by making efficient use of the parking supply and including as many spaces as feasible in a centrally-located, common pool of shared, publicly available spaces.

Action 11.4.13: Require new multi-family/mixed-use developments to provide common transportation amenities such as transit passes, bicycles, personal mobility devices, scooters, neighborhood-use electric vehicles, and shared cars as part of the development and in numbers proportionate to the size of the development.

Action 11.4.14: Work with Gold Coast Transit and VISTA to re-design bus routes and generally improve transit service to reflect the re-direction of growth patterns in Saticoy and Wells.

Action 11.4.15: Integrate and increase bus transit and pedestrian/bicycle routes with future rail transit in Old Town Saticoy. Provide bus stops as close as possible to the train station in order to provide easy transfers.

**Northwest Neighborhood Actions**

Action 11.4.16: Reconfigure Telegraph Road into a greened parkway, with bicycle lanes, and parallel parking.

Action 11.4.17: Reconfigure Telegraph Road between

Nevada and Saticoy Avenues into a greened parkway with a travel lane, a bicycle lane, and parallel parking on the south side, and one travel lane and a linear park with a bike lane on the north side.<sup>1</sup>

Action 11.4.18: Create a landscaped, pedestrian friendly east-west street from Wells Road to Saticoy Avenue that crosses the boundary between the existing mobile home park and adjacent residential tract.

Action 11.4.19: Open the cul-de-sac on South Linden Drive to connect to the planned east-west street for pedestrians (see Action 11.4.6).

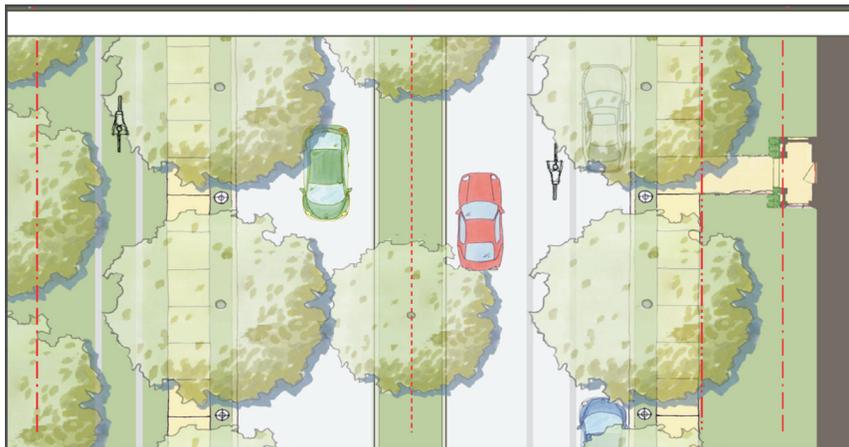
Action 11.4.20: Enhance pedestrian experience of the existing Wells Road bridge over Highway 126 through landscaping or other design features or consider a separate bridge to establish pedestrian continuity along the Brown Barranca from north to south.

Action 11.4.21: Reconfigure Wells Road between Telegraph Road and Carlos Drive as a pedestrian parkway, with central median, single-lanes, bicycle path, and parallel parking.<sup>1</sup>

**West Neighborhood Actions**

Action 11.4.22: Enhance Darling Road west of Wells Road with streetscaping and pedestrian amenities to celebrate it as an improved east-west connector.

Action 11.4.23: Enhance Saticoy Avenue and its overpass with streetscaping as an improved pedestrian-friendly connection between the neighborhoods north and south of Highway 126.



**Conceptual reconfiguration of Telegraph Road between Wells Road and Nevada Avenue**



**Illustration of Potential Development Along Wells Road**



**Illustration of Potential Street Improvements**

**East Neighborhood Actions**

Action 11.4.24: Require infill projects north and south of Darling Road to create a pedestrian friendly interconnected block-street network.

Action 11.4.25: Study the feasibility of providing a pedestrian overpass between the East and Northeast Neighborhoods in order to link them and provide increased accessibility from the north to the south.

Action 11.4.26: Create a new street north of Darling Road, and east of Wells Road, to run north-south between two infill sites.

Action 11.4.27: Establish an east-west street connection between the planned Broome and Aldea Hermosa projects with planned parks and trails.

Action 11.4.28: Consider a first phase slip road along Wells Road with angled parking on one side and parallel parking on the other to ensure that this portion of Wells Road can have retail facing it.

**Southwest Neighborhood Actions**

Action 11.4.29: Study the feasibility of a new pedestrian rail crossing.

Action 11.4.30: Extend and reconfigure North Bank Drive as a parkway with a continued center median to connect to Nardo Street.

Action 11.4.31: Create a street connection between Daffodil Avenue in the existing neighborhood and infill development south of North Bank Drive.

**Southeast Neighborhood Actions**

Action 11.4.32: Work with the County of Ventura to encourage changes to Los Angeles Avenue in Old Town Saticoy to a narrower (with parallel parking) thoroughfare.

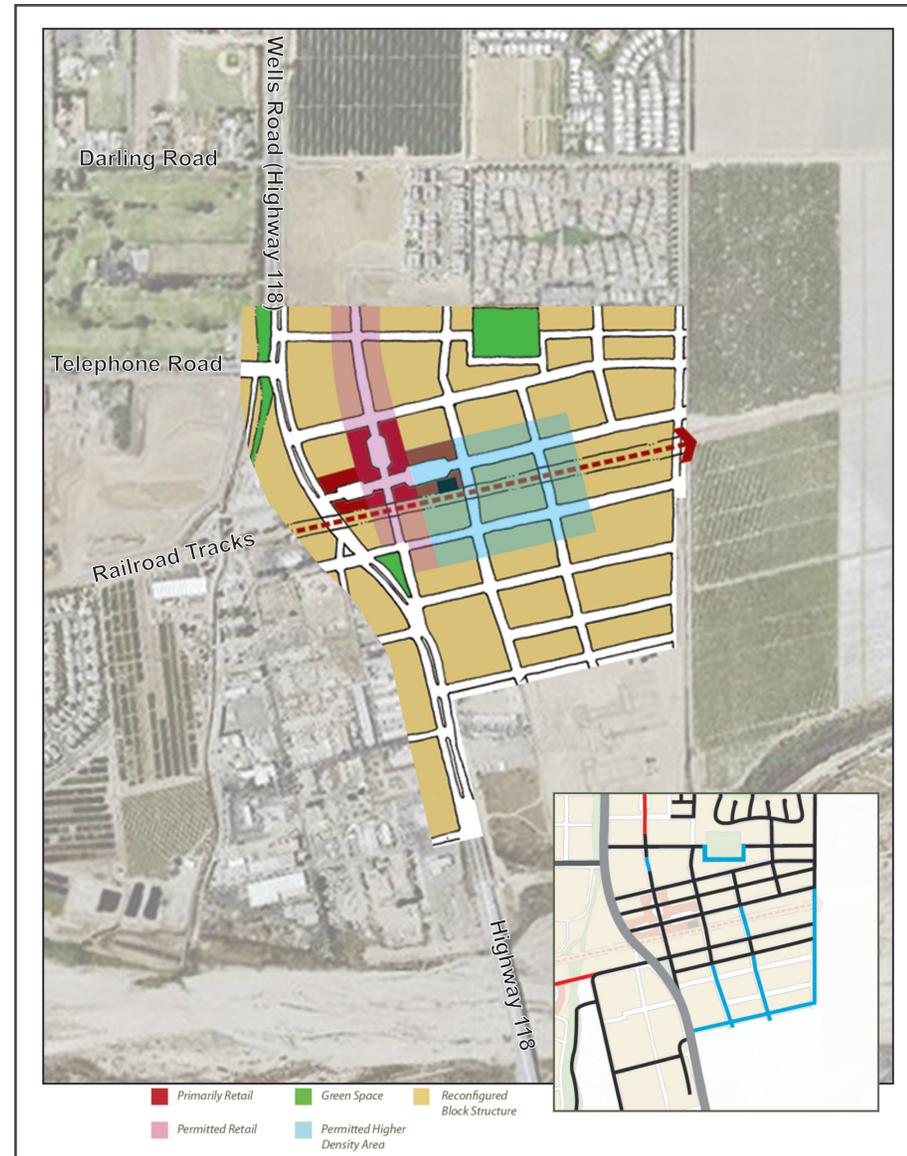
Action 11.4.33: Reconfigure Los Angeles Avenue around the rail station with on-street parking and varied shop fronts.

Action 11.4.34: Study the feasibility of providing a pedestrian crossing from Amapola Avenue south over the rail tracks.

Action 11.4.35: Reconfigure Los Angeles Avenue north of the rail hub from the current angled parking configuration into a narrower parallel parking thoroughfare with live-work frontages on the ground floor.

Action 11.4.36: Study the feasibility of extending Azahar Street west to connect to Wells Road.

<sup>1</sup> The 2005 General Plan classifies Telegraph and Wells Roads as Secondary Arterials such classifications are retained in order to assure the potential for the streets to be expanded to four-lane roads in the future. Nevertheless, the General Plan allows these particular road segments to be maintained and enhanced as two-lane roads as described in this Community Plan until traffic volumes necessitate the conversion to a four-lane road. In the event that future traffic volumes necessitate the conversion of these streets to 4-lanes, the street cross sections shall be as then determined by the Community Development Department.



**Concept for Old Town Saticoy**

### **11.5 Our Sustainable Infrastructure**

California planning guidelines define sustainable development as an integrated, systems approach to development, which attempts to maximize the efficient and effective long-range management of land, community, and resources (State of California, *General Plan Guidelines 2003*).

A sufficient water supply, an effective waste water treatment system, and an efficient drainage system are vital components of a community's well being. It is the responsibility of the City to ensure that growth does not outpace the ability to provide adequate public facilities and services.

In addition to traditional infrastructure systems, the city recognizes the value of "natural infrastructure" systems including healthy soils, vegetation, and watersheds. With this plan, the city will strive to continue to advance sustainable planning and design practices to minimize the impacts of development on natural systems and processes. The City of Ventura will incorporate practices for integrating watershed protection, water resource management, and land use planning to enable a "sustainable urbanism".

#### **Water Supply**

Under existing City policy, new development is required to conduct evaluations of the existing water distribution system, pump station and storage requirements in order to determine if there are any systems conveyance deficiencies are identified or improvements are needed, new development should not be allowed until improvements are completed.

Properties within the Plan Area that are not annexed and are outside the City Limits must conform to the City's Extraterritorial Water Service Policy in

order to receive City water service for the proposed development (Municipal Code § 22.110.055).

#### **Wastewater Treatment**

New development to be served by the City is required to conduct sewer collection system analysis to determine if downstream conveyance facilities are adequate to handle the proposed development. If additional capacity is needed, new development should not be allowed until the needed capacity is provided.

Properties within the Plan Area that are not annexed and are outside the City Limits cannot discharge to the City. City Resolution No. 76-2310 states that community sewer service shall not be provided to properties located outside the City's boundaries. However, portions of the unincorporated area are served by the Saticoy Sanitary District. Those areas that are tributary to Saticoy Sanitary District should seek approval from the District for the proposed development.

#### **Storm Drainage Systems**

Drainage patterns in the Plan Area begin in the hills north of the City and terminate in the Santa Clara River through a system of barrancas. The Franklin Barranca forms the eastern Plan boundary, and the Brown Barranca passes through the Plan Area roughly along Wells Road.

The Ventura County Watershed Protection District (VCWPD) has jurisdiction over the maintenance of the barrancas, including the provision of adequate hydraulic capacity. Both the Brown and Franklin Barranca were identified in a 1996 Capital Improvement Deficiency Study as needing improvement to support future growth in the Saticoy and Wells areas.

Manmade storm drainage systems should be planned and retrofitted to utilize City preferred engineering and design strategies that build upon existing natural systems and integrate new “green infrastructure” into the urban landscape. Low impact development best management practices such as bioswales, rain gardens, infiltration trenches, tree planters, amended soils, and permeable paving as well as larger “green infrastructure” systems such as linear parks and natural or constructed wetlands are among the City’s preferred alternative for new developments and redevelopment to meet stormwater treatment requirements and reduce stormwater impacts. These practices should support infill and redevelopment goals as well as compact community form, which are the primary means of minimizing impervious cover and maintaining permeable lands to prevent and reduce stormwater impacts. The selection of best practices for stormwater should be aligned with the development context.

**Capital Improvement Deficiency Study**

In August 1990, the City Council adopted Resolution 90-87 establishing the Wells and Saticoy Capital Improvement Deficiency Study (CIDS), which identified public improvement deficiencies and determined which parcels should contribute to or build such improvements. In 1996, upon the completion of plans addressing both sewer and water, the CIDS was updated to reflect these studies. In November of 1996, the City Council adopted a revised CIDS.

The CIDS addresses water, wastewater, storm drains, traffic system, parks system, and fire protection improvements only. Improvements to the traffic system include traffic signals at Telegraph Road and Saticoy Avenue, and at Citrus Drive and Wells Road. Additional traffic system improvements address the

bridge crossing at Darling and Wells Roads, and the striping of Wells Road (between HWY 126 and the Santa Clara River Overcrossing) to six lanes, and the extension of North Bank Drive. For parks, the CIDS establishes a fee to be used for the construction of parks and not for acquisition which is funded through Quimby fees.

This Community Plan calls for improvements to the circulation system overall, including the connectivity of thoroughfares, potential pedestrian crossings, and streetscape improvements along both Wells Road and Telegraph Road. It is anticipated that street connectivity will be achieved during the entitlement processes of new developments. However potential pedestrian crossings and streetscape improvements need to be addressed. This Plan calls for an additional update to the CIDS to reflect the proposals of this Plan so that an appropriate mechanism to fund and construct such improvements can be identified.

**Policy 11L: Promote the use of existing, natural systems for resource management.**

Action 11.5.1: Require new development to maximize and preserve permeable land surfaces, to the extent feasible, for water quality protection, groundwater recharge, flood prevention and watershed health.

Action 11.5.2: Make use of existing barrancas for drainage, and utilize other naturalistic features such as bioswales, ponds, and wetlands to capture and treat runoff, decrease flooding, and recharge groundwater. Comply, at a minimum, with the current municipal National Pollutant Discharge Elimination System requirements for peak flow, stormwater quality, and runoff volume and hydromodification.

Action 11.5.3: Work with the Saticoy Sanitary District (SSD) to study the feasibility of extending the



Permeable Parking Surface

reclaimed water infrastructure throughout the Saticoy & Wells Plan Area for landscape irrigation.

**Policy 11M: Provide adequate public facilities and services to serve new development and maintain current services.**

Action 11.5.4: New development shall provide adequate public services and facilities as determined through the development review process.

Action 11.5.5: Update the 1996 *Capital Improvement Deficiency Study (CIDS) for the Saticoy and Wells Communities*.

Action 11.5.6: Require new development to either pay their proportionate share for or construct specific improvements identified in the updated *Saticoy and Wells Capital Improvement Deficiency Study and the Citywide Water and Wastewater 2009 Master Plan* (see Action 11.5.5 above).

Action 11.5.7: Amend Municipal Code Sec. 22.110.055 for water connections outside city limits to establish criteria for providing water to the unincorporated areas of Saticoy. Consult with the County of Ventura and other appropriate entities.

## 11.6 Our Active Community

### Parks

Parks provide meeting places where residents and visitors share the area's natural amenities and enjoy recreational activities and cultural events. Recognizing the importance of parkland development, the City of Ventura has adopted policies that provide for the incorporation of parks and open spaces.

### Community Facilities and Recreation

The City values the social benefit of youth-oriented programs such as tutoring, recreation, education, and classes focused on the arts. Through partnerships with the Ventura Unified School District and the community organizations, the City of Ventura provides after school programs, including the Program Enrichment for After School Kids (PEAK). In addition, non-profit entities such as the Boys and Girls Club and the YMCA provide services to the immediate residents in which they are located. Such organizations provide a safe environment for supervised athletic, educational and creative activities for a broad range of ages. As existing and new neighborhoods evolve within the larger community, additional or expanded facilities and programs are desired to meet community needs and maintain existing service to residents.

Following City policies and this Community Plan, several planned and potential developments in the Plan Area have made or will make provisions for park development and new community facilities. Furthermore, the County owns the land on which the Saticoy Regional Golf Course sits and currently leases it to a private operator. The City, in conjunction with the school district, County and residents, may have the opportunity to consider the conversion of this location into a public school, or other public or civic facility

with joint use agreements for additional open space and other civic uses, such as community centers, expanded library facilities, and senior services.

### Open Space Strategies

The various open spaces in the Saticoy and Wells area, while presenting significant opportunities, are fragmented from each other. As such their interconnection becomes paramount to the overall vision of the place. Figure 11-11 shows an envisioned open space network for the Plan Area. It involves the enhancement of existing natural features, and the integration of existing neighborhood parks with proposed parks through a variety of well-designed streets. This open space armature, together with various civic facilities, becomes the communal thread that binds the neighborhoods together.

The open space strategies are as follows:

1. Respect the surrounding agricultural fields through positive frontages, thereby creating a friendly urban edge towards them.
2. Celebrate the barranca as an ecological and community amenity through its preservation and enhancement.
3. Create a new park along the Santa Clara River bank. New development along this park should create a positive face towards it.
4. Enhance the Santa Clara River edge through ecologically sensitive design and native planting.
5. Consistent with General Plan Action 6.11, update standards to allow for a variety of park sizes and types.

Existing park and recreation facilities located in the Community Plan Area:

- Saticoy Community Park, known as the "Old Ball Field"
- Fritz Huntsinger Youth Complex
- Saticoy Regional Golf Course (requires entrance fee)



**Saticoy Community Park**

6. Reconfigure Wells Road as a parkway with a distinct character that encourages pedestrian life.
7. Create a new linear park along Telegraph Road to improve the aesthetic character of the tracts to the north.
8. Create a network of great streets to interconnect the various open spaces. A variety of streetscapes need to be generated through a diversity of tree types, sidewalk character, and street furniture.
9. Enhance the area around the rail tracks in Old Town Saticoy as a linear promenade that can evolve into a vibrant transit hub with mixed-use development.
10. Preserve the historic Chumash burial grounds as a significant community amenity as well as a memory of the history of the Saticoy and Wells area.
11. Preserve and augment existing parks through direct street connections so they are easily accessible from surrounding neighborhoods.
12. Create multi-functional parks and open space that serve individuals, the community, and the environment.

**Policy 11N: Develop a rich and interconnected palette of public open spaces in an inspirational manner that facilitates social interaction and a sense of community, and provides ecoservices such as planned sub-basin drainage and storage.**

Action 11.6.1: Require new smaller open spaces, including public plazas, fountains, and pocket parks, on portions of blocks to supplement larger public open spaces and to diversify the built environment.

Action 11.6.2: Allow for a range of park types in a variety of configurations and sizes no less than 5,000 square feet.

Action 11.6.3: Design and configure parks to include connectivity amongst one another, including, but not limited to, directional signage or other key features.

Action 11.6.4: In anticipation of a new rail station, plan for a plaza or central green near the historic train depot.

Action 11.6.5: Require street trees as an essential element of the open space network that connect trails, parks, and other natural areas.

Action 11.6.6: Developing linear shared-use pathways along the Brown and Franklin Barrancas and Santa Clara River. Work with appropriate agencies, such as the Ventura County Flood Control District.

Action 11.6.7: Revitalize the Saticoy Community Park to improve design, access, and use opportunities. Amend the General Plan to designate this site as Parks and Open Space.

Action 11.6.8: Coordinate with the County of Ventura, to explore public school, and joint use agreements for public parks and civic uses, community facilities such as youth and senior centers, library, community center, etc., and new open spaces on the site of the Saticoy Regional Golf Course.

Action 11.6.9: Extend a linear park along the Brown Barranca southward to create a new green edge

along the Santa Clara River and to wrap the proposed neighborhood in this area with green space.

Action 11.6.10: Create a neighborhood park as a transition element between the North Bank project and the existing housing tract to the north.

Action 11.6.11: Create multi-functional parks and open space that benefit people and the environment by protecting and enhancing water supplies, and providing flood and storm water management services.

Action 11.6.12: Identify opportunities to use and connect public lands such as playing fields, parks, and rights-of-way for “green solutions” to water quality and supply problems, while creating a more human urban environment.

**Policy 110: Design community facilities to provide multiple community benefits, including daytime activities for seniors, weekend athletic programs requiring a gymnasium, a community theater venue, and community-based meeting space.**

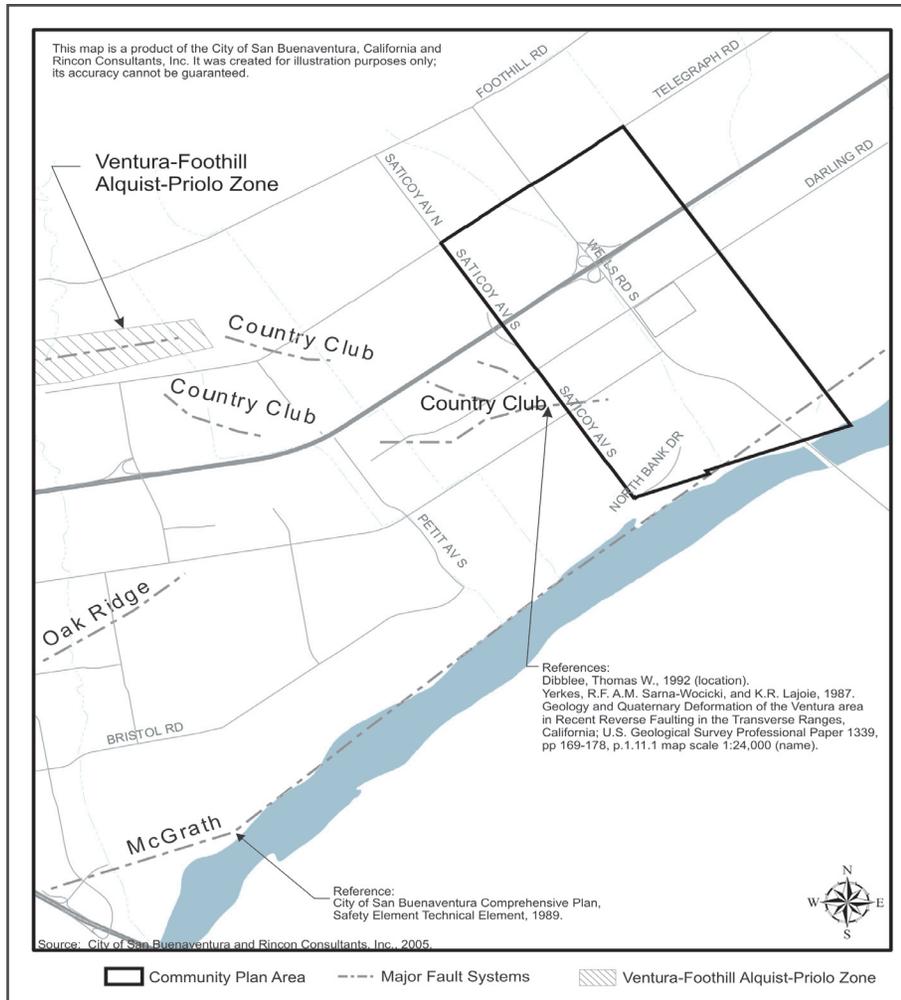
Action 11.6.13: Centrally locate new community facilities with safe, easily accessed pedestrian and bicycle connections to existing and future neighborhoods.

Action 11.6.14: Place new facilities within or adjacent to City parks and/or schools.

Action 11.6.15: Work with the Ventura Unified School District and the community organizations to provide youth services, such as PEAK, to area residents.



Figure 11-11 - Conceptual Green Network (exact locations will be determined through the entitlement process)



**Figure 11-12 - Seismic Hazards**

### 11.7 Our Healthy and Safe Community

Cumulatively, there are significant natural and man-made risks in the Plan Area, which must be planned for appropriately in order to create a safe and healthy urban environment. The Saticoy Industrial District is particularly susceptible because of the combined risks of dam inundation, flooding, liquefaction, and earthquake faults. Potential risks to life and property must be minimized through innovative programs, model mitigation measures, and mindful urban design.

#### Flood Control

A large portion of the community lies within the 100- and 500-year floodplain, which functions as a natural drainage area from the hillsides to the Santa Clara River and its surrounding broad floodplain (see Figure 11-5).

Land uses can substantially increase chances of flooding. Alterations to the drainage basin or floodplain, growth of brush and trees, denudation of vegetation (including fire damage), construction of impervious surfaces, channelization of water courses, and installation of bridges and other blockages at stream crossings, all contribute to flood susceptibility.

The community also has several upstream dams, and a broad section of Saticoy and Wells is in a potential dam inundation area, which requires good emergency preparedness.

#### Seismic Activity

The Plan Area has two faults that cross its boundaries at the southern edge near industrial lands – the Country Club Fault and the McGrath Fault (see Figure 11-12). The San Andreas Fault poses a threat as well. Located in a highly active earthquake region, the area is subject

to geological hazards, such as ground shaking, surface rupture, liquefaction, and soil subsidence.

**Noise Factors**

Saticoy and Wells receives considerable noise from regional traffic on Highway 118 (Wells Road) and Highway 126, major arterials that are in close proximity to residences and outdoor activities. Further noise issues arise along the edges of the Plan Area from the agricultural activities, where heavy-equipment use potentially conflict with urban functions.

**Police, Fire, and Emergency Preparedness**

The City goal for police response time for emergency calls is within five minutes, 90% of the time. Currently, the Police Department is able to respond within five minutes, 50% of the time. Without additional staffing the response times for Saticoy and Wells will be significantly worse, as the Plan Area is at the far east end of one of the Police Department patrol areas. The Police Department is currently below the national average of 1.5 officers per 1,000 residents and would require an additional 25 officers to reach that standard for the City. Efforts to increase police staffing are being pursued on a citywide basis. The Police Department views additional personnel, rather than additional facilities, as the appropriate means to provide adequate coverage to all parts of the City, including the east end of Ventura.

The City is currently below the state average (0.98 per 1,000 residents) for firefighters. An additional 29 firefighters are need citywide to reach the state average based on the current population. Given the lack of emergency personnel, the Fire Department currently achieves its emergency response time goal of four minutes on 68% of emergency calls. Within the Plan Area is Fire Station Number 8, located on Darling Road, east of Wells Road. The facility

provides the area with close access, however providing increased firefighter personnel continues to be pursued on a citywide level.

Impacts on police and fire are evaluated during the entitlement process of new developments through CEQA review, and are required to provide appropriate mitigation.

**Policy 11P: Maintain safe and enjoyable neighborhoods by mitigating risks posed by natural hazards.**

Action 11.7.1: Review the integrity of barranca structures to evaluate hazards to adjoining development from failing or disintegrating barranca walls.

Action 11.7.2: Monitor the use and storage of hazardous substances in the Saticoy Industrial District to alleviate the risk of watercourse contamination along the Santa Clara River through development review and NPDES monitoring requirements.

Action 11.7.3: Profile the source of natural hazards in the Community Plan Area and develop specific emergency response measures.

**Policy 11Q: Develop a safety-by-design strategy that employs, where feasible, urban design techniques to reduce crime activities.**

Action 11.7.4: Integrate features such as public visibility, night-time public use, low-level lighting, or other prevention measures, into the design of commercial and public buildings in order to create a safe environment, particularly in mixed-use areas.

action 11.7.5: As resources become available, add code enforcement personnel in order to provide code enforcement in public places, recognizing the relationship of crime to poorly maintained areas.



**Fire Station Number 8**

Two public schools and two private school are within the Saticoy & Wells Community Plan Area:  
(1) Saticoy Elementary School;  
(2) Douglas Penfield Special Education School;  
(3) Sacred Heart School; and  
(4) St. Augustine Academy



**Saticoy Library**

### **11.8 Our Educated Community**

The majority of students in the Saticoy & Wells Community Plan Area attend Saticoy Elementary School, Balboa Middle School, and Buena High School. These schools are near capacity, and will require either expanded on-site improvements, or built facilities in new areas.

It is important to support a well-educated, active, and culturally sensitive community. Providing a variety of learning opportunities enriches the lives of children and helps to assure their economic independence. To meet the growing needs of the community, a potential expansion or relocation of the Saticoy Library can also offer increased educational opportunities.

**Policy 11R: Work with the Ventura Unified School District to provide for adequate public schools and learning centers to meet expected growth in the Saticoy & Wells Community Plan Area.**

Action 11.8.1: Work with the Ventura Unified School District to ensure that school facilities are provided to serve new development in Saticoy and Wells.

Action 11.8.2: Develop joint-use community facilities for schools, seniors, and other programs in the Community Plan Area near the populations with the greatest need.

Action 11.8.3: New development proposals and City Thoroughfare enhancements should link new and existing school sites into a cohesive network of pedestrian-friendly streets, trails, paths, and bikeways for safe public access.

## 11.9 Our Creative Community

The arts build civic pride, create community identity, and demonstrate respect for cultural diversity. This is particularly true when community arts and programs are built on local historical foundations and cultural contributions of residents. To weave the arts into everyday life and enhance the newly designed areas, generous provisions for public art should be made.

Protecting the cultural heritage and promoting creativity and civic art in the Saticoy and Wells communities will not only improve the quality of the built environment, but can also animate the public realm through festivals and cultural activities.

**Policy 11S: Meet the diverse creative needs of the community by engaging residents in art and culture programs.**

Action 11.9.1: Promote civic art activities distinctive to Saticoy, such as local art exhibits, art education opportunities, concerts, neighborhood festivals, and other special events.

**Policy 11T: Provide adequate public and private infrastructure, development standards, business incentives, and funding for the local development of art and culture in the Community Plan Area.**

Action 11.9.2: Consistent with the City's Annual Public Art Work Plan, collaborate with the Public Works Departments and the Public Art Commission to integrate artwork in public works projects in the Plan Area.

Action 11.9.3: Identify cultural space needs in the Saticoy & Wells Community Plan Area, including classroom space, exhibition space, public art locations, and live/work space for local artists.

Action 11.9.4: Make generous provisions for public

landmarks and public art in civic and neighborhood centers. Public art can include, but is not limited to, site-specific murals, sculptures, and artist-designed functional amenities.

Action 11.9.5: Encourage locating public art and culture facilities in neighborhood centers through developer incentives, such as exempting non-profit art facility space on the ground level of buildings from certain development standards with recorded agreements requiring perpetuity of the cultural use.

**Policy 11U: Enhance, preserve, and celebrate the historic and prehistoric resources.**

Action 11.9.6: Preserve the historic Chumash burial grounds as a significant community amenity as well as a memory of the history of Saticoy and Wells.

Action 11.9.7: Prior to annexation conduct a historical survey (in Old Town Saticoy and other areas as appropriate) in accordance with the U.S. Department of the Interior: Guidelines for Local Surveys.

Action 11.9.8: Upon completion of a historical survey, all new development on a lot containing a historic resource to be reviewed by the Historic Preservation Committee for compliance with the Secretary of the Interior's Standards and Guidelines for the treatment of Historic Properties.

Action 11.9.9: Upon completion of a historical survey, require development contiguous to a lot containing a historic resource to be reviewed by the Design Review Committee for compliance with the City's Municipal Code and the Historic Preservation Committee for compliance with the Secretary of the Interior's Standards and Guidelines for the Treatment of Historic Properties.

Action 11.9.10: Establish a permit application fee for review by the Historic Preservation Committee.



Action 11.9.11: Update the Historic and Prehistoric Sensitivity map every 10 years.

### **11.10 Our Involved Community**

Saticoy and Wells will build its future through community participation that allows its diverse and growing population access to the decision-making process.

Implementation of the Community Plan should continue in the spirit of the Plan preparation. That is, with the active participation of local residents working in conjunction with City staff and elected officials.

#### **Policy 11V: Develop strong community outreach programs and services to improve public participation and decision-making.**

Action 11.10.1: Establish places for civic discourse and community interaction in the planned public-use environment for Saticoy and Wells.

Action 11.10.2: Work with the Saticoy Community Council and the East Ventura Community Council to address evolving land use, economic, and neighborhood issues on the east side.

Action 11.10.3: Actively seek representation of Saticoy and Wells residents on City boards, committees, and commissions to encourage more participation by eastside residents in Citywide policy and program decisions.

**APPENDIX A**

**PUBLIC INFRASTRUCTURE IMPROVEMENTS**

Note: A citywide water and wastewater Master Plans are currently under preparation. Deficiencies in the Wells and Saticoy Communities may be identified during the development review process or through the citywide Master Plans. Development projects in the Community Plan area that have not received their entitlements to date may be required to construct specific improvements or pay their pro rata share to rectify these new deficiencies. This would be in addition to the requirements outlined in the Wells and Saticoy Communities Capital Improvement Deficiency Study.

**1. IMPROVEMENTS NOT YET CONSTRUCTED**

**a. Water System Improvements**

- (1) Saticoy Well #3 - Programmed as a Work Plan project in the 2009-2015 Capital Improvement Project Plan; construction likely in 2010-2011; Funding sources: Water Enterprise funds & CIDS fees
- (2) 12" main east of Wells and south of Freeway 126 – Not programmed; Funding sources: Water Enterprise funds & CIDS fees
- (3) 12" main Los Angeles Avenue - Not programmed; Funding sources: Water Enterprise funds & CIDS fees
- (4) 12" main North Bank Drive west of Saticoy Avenue - Not programmed; Identified as a Potential project in the 2009-2015 CIP Plan; Funding sources: Water Enterprise funds & CIDS fees
- (5) 12" main North Bank Drive east of Saticoy Avenue - Not programmed;

Identified as a Potential project in the 2009-2015 CIP Plan; Funding sources: Water Enterprise funds & CIDS fees

- (6) 12" main Citrus/Darling Road - Not programmed; Funding sources: Water Enterprise funds & CIDS fees

**b. Wastewater System Improvements**

- (1) Western trunk sewer – Harper lift station to North Bank Drive lift station - Identified as a Potential project in the 2009-2015 Capital Improvement Project Plan; Funding sources: Wastewater Enterprise funds & CIDS fees
- (2) Southern trunk sewer – Saticoy Avenue to Brown Barranca – One segment completed as part of new development; remaining segment not programmed; Funding sources: Wastewater Enterprise funds & CIDS fees
- (3) Southern trunk sewer – Brown Barranca to Los Angeles Avenue – Not programmed; Funding sources: Wastewater Enterprise funds & CIDS fees

**c. Storm Drain System Improvements**

- (1) Brown Barranca Reach 1 - Design and construction by the County of Ventura; Not programmed; Funding sources: County funds & CIDS fees
- (2) Brown Barranca Reach 2 - Design and construction by the County of Ventura; Not programmed; Funding sources: County funds & CIDS fees
- (3) Brown Barranca Reach 3 - Design and construction by the County of Ven-

tura; Not programmed; Funding sources: County funds & CIDS fees

- (4) Brown Barranca Reach 4 - Design and construction by the County of Ventura; Not programmed; Funding sources: County funds & CIDS fees
- (5) Saticoy Drain - Design and construction by the County of Ventura; Not programmed; Funding sources: County funds & CIDS fees
- (6) Franklin Barranca - Design and construction by the County of Ventura; Not programmed; Funding sources: County funds & CIDS fees
- (7) Santa Clara Levee - Not programmed;
- (8) Darling Road Storm Drain - Not programmed; Funding sources: City General fund & CIDS fees

d. Traffic Improvements

- (1) Highway SR-118 - Not programmed; Funding sources: CIDS fees
- (2) North Bank Drive from Los Angeles Avenue to Brown Barranca - Not programmed; Identified as a Potential project in the 2009-2015 CIP Plan; Funding sources: City funds & CIDS fees
- (3) North Bank Drive from Brown Barranca to Cabrillo Village - Not programmed; Identified as a Potential project in the 2009-2015 CIP Plan; Funding sources: City funds & CIDS fees
- (4) Los Angeles Avenue extension be-

tween Violetta Street to Aster Street – Concurrent with Broome property development; Funding sources: CIDS fees

- (5) Telegraph Road between Saticoy Avenue and Wells Road - <sup>1</sup> Estimated Cost \$1,650,000; No identified source of funding other than the additional funds proposed to be contributed by the Parklands project
- (6) Wells Road between Telegraph Road and Carlos Street - <sup>1</sup> Estimated Cost \$750,000; No identified source of funding other than the additional funds proposed to be contributed by the Parklands project

e. Park Improvements

- (1) Wells Community Neighborhood Park - To be developed concurrent with UC Hansen property development as part of a proposed developer agreement; Funding sources: Quimby & CIDS fees
- (2) Saticoy Community Neighborhood Park – Location not identified; likely to be developed concurrent with development of vacant property along Santa Clara River; Funding sources: Quimby & CIDS fees

2. CONSTRUCTED IMPROVEMENTS

a. Water System Improvements

- (1) Elizabeth Reservoir - constructed by developers
- (2) 12" main north of Freeway 126 and west of Wells Road -constructed by

City as a capital improvement project (Spec 2005-23)

- (3) 12" main south of Telephone Road and west of Brown Barranca - constructed by developer – Reimbursement Agreement pending

b. Wastewater System Improvements

- (1) North Bank Drive lift station - constructed by City as a capital improvement project (Spec 2002-012)
- (2) North Bank Drive force main - constructed by City as a capital improvement project (Spec 2004-07)
- (3) Wells Road trunk sewer - constructed by developer – Reimbursement Agreement pending
- (4) Southern trunk sewer between Saticoy Avenue and North Bank lift station - constructed by developer – Reimbursement Agreement pending
- (5) Abandon Wells Road lift station and force main - completed by City
- (6) Bristol relief sewer - constructed by City as a capital improvement project (Spec 98-09)

c. Storm Drain System Improvements

- (1) Saticoy Avenue storm drain - constructed by City as a capital improvement project (Spec 90-05)
- (2) Sudden Barranca - constructed by developer – Reimbursement Agreement No. 98-003

d. Traffic Improvements

- (1) Telegraph Road and Saticoy Avenue traffic signal - constructed by City as a capital improvement project (Spec 98-08)
- (2) Wells Road and Citrus Drive traffic signal - constructed by developer – Reimbursement Agreement No. 2000-55

<sup>1</sup> Improvement not included in the Wells & Saticoy Communities Capital Improvement Deficiency Study