

Westside

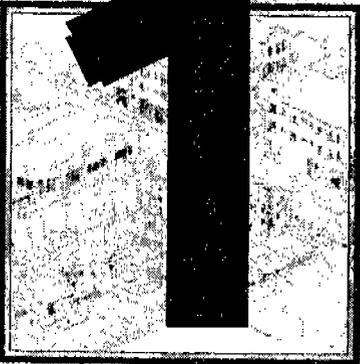
URBAN DESIGN PLAN

January 20, 1999

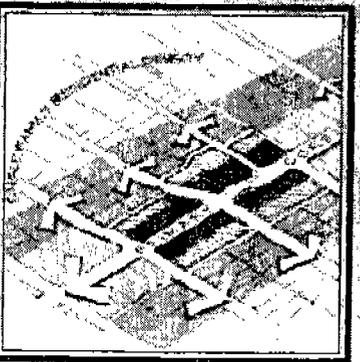
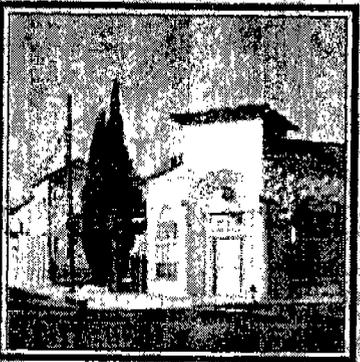
Prepared for the Westside Community
and the City of Ventura
by
Mainstreet Architects & Planners, Inc.
with
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Introduction



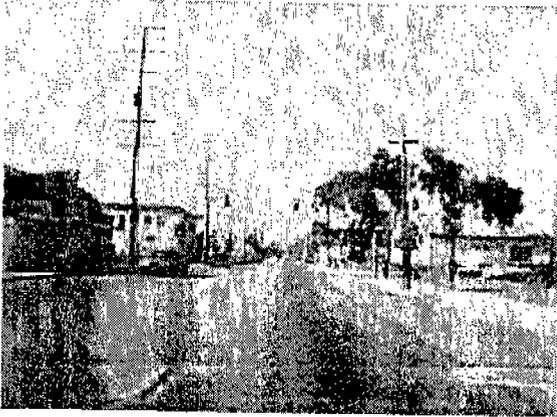
AN URBAN DESIGN PLAN FOR THE WESTSIDE

The initial Request for Proposals for the Westside Urban Design Plan stated that the overriding goal for this community design effort would be to “enhance the aesthetic environment and create a sense of place for the Westside Community.” But the people of the Westside Community know that it is many of the existing differences – among them the narrow streets, diverse layers of uses, multi-cultural influences and rich older building stock – which, even now, combine to make the Westside Community a wonderful place to live. With this understanding, perhaps it could be stated more accurately that the primary goal of this effort is *to reinforce and enhance* the unique historic, cultural and geographic qualities – *the sense of place* – that already exists, and gives the Westside Community a distinct neighborhood character within the City of Ventura.

“...the primary goal of this effort is to reinforce and enhance the unique historic, cultural and geographic qualities – the sense of place – that already exists, and gives the Westside Community a distinct neighborhood character within the City of Ventura.”



The Westside Community is compact and diverse - key ingredients in creating “community sustainability.”



Historically, Ventura Avenue has been one the City's most active and vital business corridors .



Development along Ventura Avenue is diverse in built form and use. Opportunities for positive urban in-fill abound.



The Westside Community is home to numerous small businesses which serve the neighborhood well .

ISSUES OF CONCERN

In accomplishing this goal, issues that have been addressed by this planning effort include:

- Confirmation of the key objectives established through the 1996 "Take Part" Westside workshops (an effort recognized nationally and recipient of the HUD Blue Ribbon Best Practice Award) which resulted in a Consensus Plan, which established the overall program for this planning effort;
- Continuation of public participation and education, which remain essential components for the successful implementation of this effort;
- Design for the public realm, including streetscape configuration, gateway monumentation, parking strategies, urban furnishings (benches, bus stops, street lighting, etc...) and other appropriate improvements;
- Building design guidelines, to reinforce the positive character of what is already built and to guide new and renovated construction to be compatible with this existing character;
- Sign guidelines, to promote the artful use of signs as the outward expression of a business' desire to be of service to neighbors and members of the community;
- An implementation strategy, identifying associated costs of the plan components, means and funding sources, priorities and implementation timelines to achieve results.

KEY AXIOMS FOR SUCCESSFUL REVITALIZATION

The issues and challenges encountered on the Westside – especially those related to property disinvestment are not exclusive to the area. In fact, as can be seen in many similar communities (especially in Southern California), rapid and outwardly expanding growth patterns, combined with shifting economic forces, have almost guaranteed that areas such as this would slip into economic uncertainty sooner or later.

The only appropriate response must be one that is based on conscious and strategic planning. An area such as the Westside Community, with its diverse residential, business, and industrial bases, must be recognized as the valuable asset that it is for the broader community. The existing community represents a tremendous investment in public infrastructure and land and, like any good investment, it must be nurtured and maintained.

The Westside Urban Design Plan has been prepared with this understanding in mind, and is based upon the following key axioms of Revitalization:

- Change is inevitable and will occur whether we plan for it or not – so we had better plan for it so that it happens consciously and on our terms;
- Property disinvestment – that is manifest in a visible lack of care or concern for property – is a symptom, not a cause, of a community's collective uncertainty about itself. Its roots are both economic and "spiritual." It is downward spiraling, and can only be countered by collective and purposeful intent;
- Little victories are just as important as big plans in achieving success in revitalization;
- Public investment is often an important first step in demonstrating commitment to property owners, but public investment alone will not turn an area around;
- *A lesson learned by the Community's creation of the plaza next to the Adult Center - don't wait for government money*



The public workshop process gave citizens an opportunity for "hands-on" input into the planning process for the Westside Community's future. That input proved essential in developing the recommended strategy.



"There is no such thing as a bad idea!" All comments and thoughts were given consideration in this public forum.

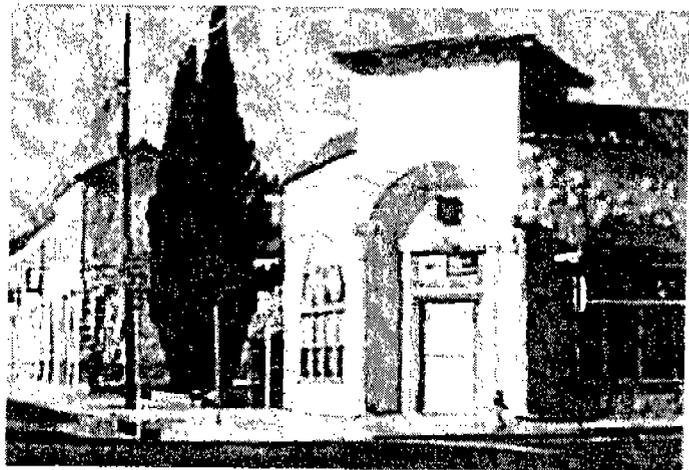


The various options and issues were ranked by participants to help the design team evaluate the community's priorities.

or outside motivation; and that the seeds for lasting change must come from within.

SUCCESS IS A SHARED RESPONSIBILITY

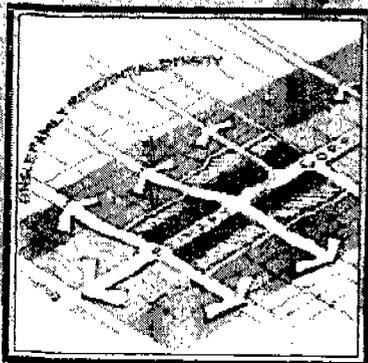
This Document was prepared with the enthusiastic participation of many citizens of the Westside and it provides the Westside Community and the City of Ventura with a Plan for guiding positive change. It is a Vision based upon common sense and proven design principles and it must be shared with residents, property owners, business people and investors, who must each assume responsibility for its success. With implementation, there should be little doubt that the Westside Community will re-emerge as a vital economic and social force within the City of Ventura in the coming years.



The architectural character of this existing Westside building is proof that we have known how to do good neighborhood development for a long time. We simply need to remind ourselves.



The Vision for Westside

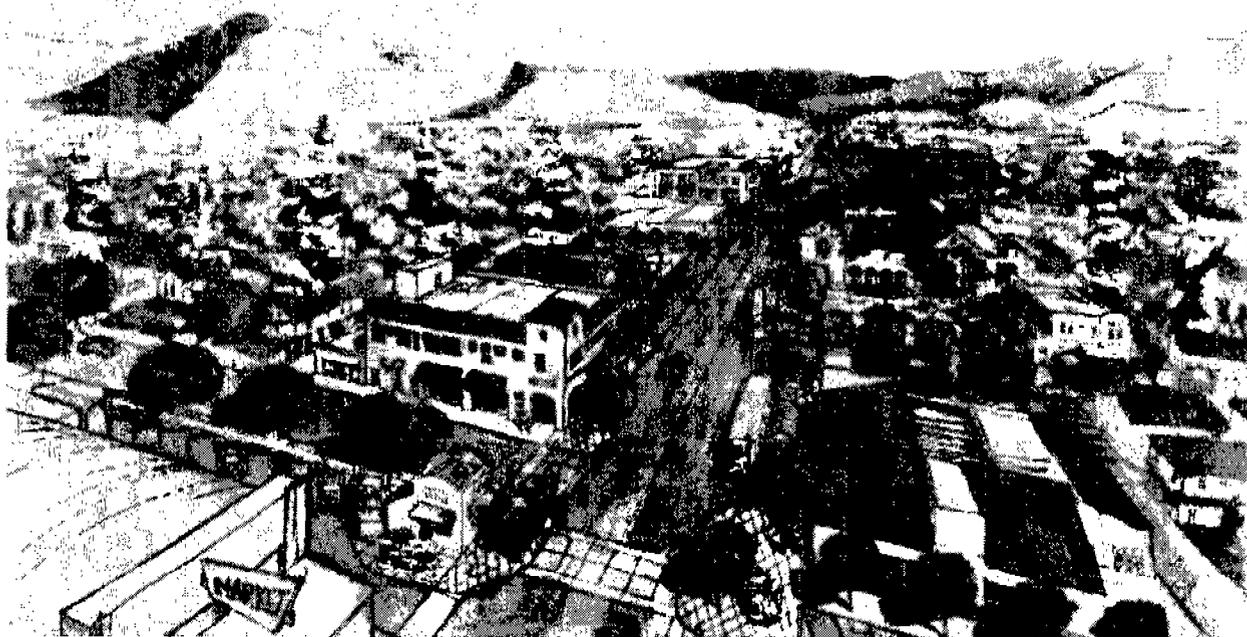


THE VISION FOR WESTSIDE

The Vision for Westside Ventura is of a place rich in diversity – culturally and economically – a tight-knit community of residents and businesses; a place where good housing is available in a mixture of types. These include cottages, bungalows, hillside homes, apartments and townhouses, all within reasonable walking distance to shops, businesses and public transportation. It is a place where owning a car is not a prerequisite to enjoying access to the benefits of the community. It is a place with a rich history and a promising future, worthy of reinvestment, as well as new investment.

This document comes on the heels of the *Westside Consensus Plan* to establish a Vision for the Westside Community and was prepared in 1996 in a workshop environment. Most significant to this effort, the plan identified the community's desire to retain and promote a local core of businesses, shops and service uses that would support the residents in a very pedestrian-friendly environment.

The Consensus Plan also identified a desire to strengthen



Bird's eye view of Ventura Avenue in the "Primary Pedestrian Core" area, where diverse uses can come together in a pedestrian-friendly, neighborhood-oriented environment.

the community's identity as a physical district, by reinforcing points of entry (as gateways) and by improving the appearance of the significant travel corridors – primarily Ventura Avenue and Olive Streets, but also Main Street and Stanley Avenue. The Consensus Plan also identified the desire to promote pedestrian and bicycle access throughout the district for both recreational and functional purposes.

Preparation of this document continued the Workshop process with an emphasis on physical improvements for both the public and private realms, and on creating a strategic physical plan to help realize the Community's vision.

“Most significant to this effort, the (Westside Consensus) plan identified the community's desire to retain and promote a local core of businesses, shops and service uses that would support the residents in a very pedestrian-friendly environment.”



Mixed-use in-fill opportunities can play a significant role in attaining the Westside Community's vision.

A STRATEGY FOR RENEWAL

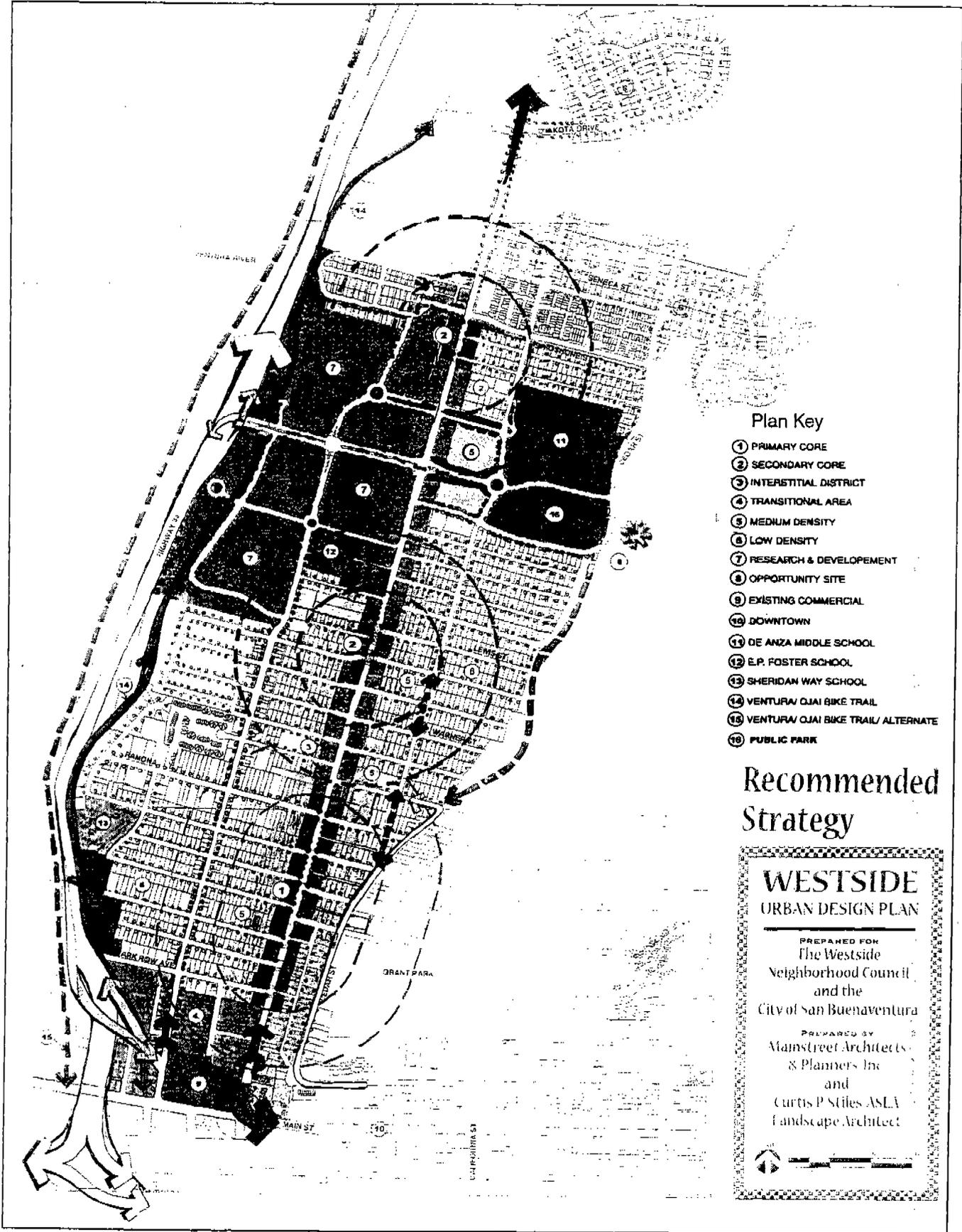
The proposed plan, along with supporting concepts, emerged out of the second public workshop as the Recommended Strategy, and is illustrated herein. Although it calls for rethinking the present distribution of development types, this strategy builds upon some of the area's traditional development patterns, combined with current thinking about mixed-uses, transit-oriented development and accessibility. This is an effort to support, both physically and economically, the positive pedestrian and urban qualities called for in the Westside Consensus Plan.

Although the main focus of this planning effort is the urban design of the public realm, the workshop process revealed concern from the community regarding the viability and appropriateness of certain uses, (especially some of the older heavy industrial and transitional uses in the vicinity of Stanley Avenue as well those in the vicinity of Fix Way). In response to these concerns, the Vision presented here identifies the opportunity to rethink these areas, and to develop a strategy for directed improvements to assure that these areas continue to improve and contribute to the long-term economic vitality of the area.

“The Vision presented here identifies the opportunity to rethink these areas, and to develop a strategy for directed change...”



Intensity of use would diminish as one moves away from the Core Areas.



Plan Key

- ① PRIMARY CORE
- ② SECONDARY CORE
- ③ INTERSTITIAL DISTRICT
- ④ TRANSITIONAL AREA
- ⑤ MEDIUM DENSITY
- ⑥ LOW DENSITY
- ⑦ RESEARCH & DEVELOPMENT
- ⑧ OPPORTUNITY SITE
- ⑨ EXISTING COMMERCIAL
- ⑩ DOWNTOWN
- ⑪ DE ANZA MIDDLE SCHOOL
- ⑫ E.P. FOSTER SCHOOL
- ⑬ SHERIDAN WAY SCHOOL
- ⑭ VENTURA OJAI BIKE TRAIL
- ⑮ VENTURA OJAI BIKE TRAIL/ ALTERNATE
- ⑯ PUBLIC PARK

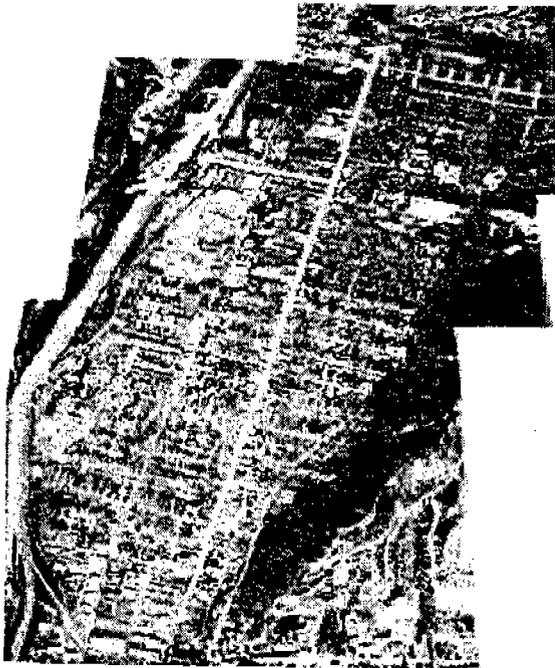
Recommended Strategy

WESTSIDE
URBAN DESIGN PLAN

PREPARED FOR
The Westside
Neighborhood Council
and the
City of San Buenaventura

PREPARED BY
Alamstreet Architects
& Planners Inc
and
Curtis P Stiles ASLA
Landscape Architect



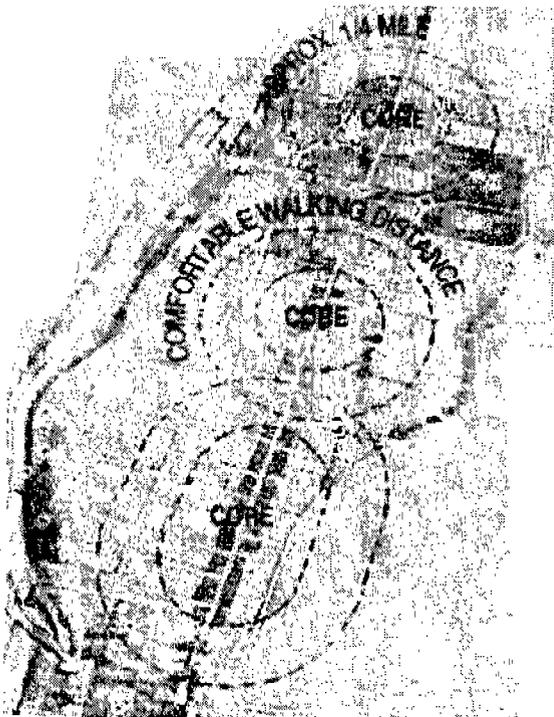


Aerial view of the Westside today.

STRATEGIC REVITALIZATION OBJECTIVES

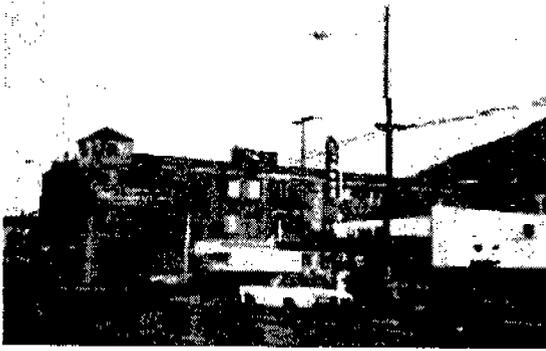
The strategy for revitalization presented here combines several important objectives, each of which will contribute to the success of the Westside:

- Recognition and support of the existing and historic built character, especially with regard to the increments of buildings, blocks and neighborhoods, which combine to make up the Westside;
- Emphasis on creating and/or bolstering (moderately dense) "pedestrian cores" comprised of business, residential and cultural/social uses, strategically placed to maximize accessibility to all citizens of the Westside Community;
- Renovation of the Public Realm to reinforce the identity of the Westside as a significant district, addressing its edges, gateways, core areas and corridors, with an emphasis on the promotion of safe vehicular access and comfortable pedestrian access throughout;
- Comprehensive and strategic planning to support and replenish the business and industrial base of the Westside, to promote orderly, safe and environmentally-balanced distribution of uses.



Aerial overlay view of the Westside, showing the recommended strategy of primary and secondary core areas.

In response to the workshop input, the Recommended Strategy calls for the establishment of several "pedestrian core areas" - areas of more intense mixed-uses and refined streetscape, along the length of Ventura Avenue, connected by interstitial ("in between") areas of less intensity. In keeping with the community's goals, the Plan establishes a primary pedestrian core area at the southerly end of Ventura Avenue. Two additional smaller pedestrian cores placed strategically along the Avenue to the North (one exists in part, one is new) provide some balance among the many neighborhood areas in terms of pedestrian access for day-to-day needs.



Ventura Avenue at Simpson Street, today.



Ventura Avenue at Simpson Street, with Core Area reconstruction, re-use or renovation as envisioned by the Plan.



A vacant parcel on Ventura Avenue, today.



An in-fill project in one of the "interstitial areas" (between core areas) of Ventura Avenue, as envisioned by the Plan.



Ventura Avenue at Ramona, today.



Ventura Avenue with street improvements and renovations, as envisioned by the Plan.



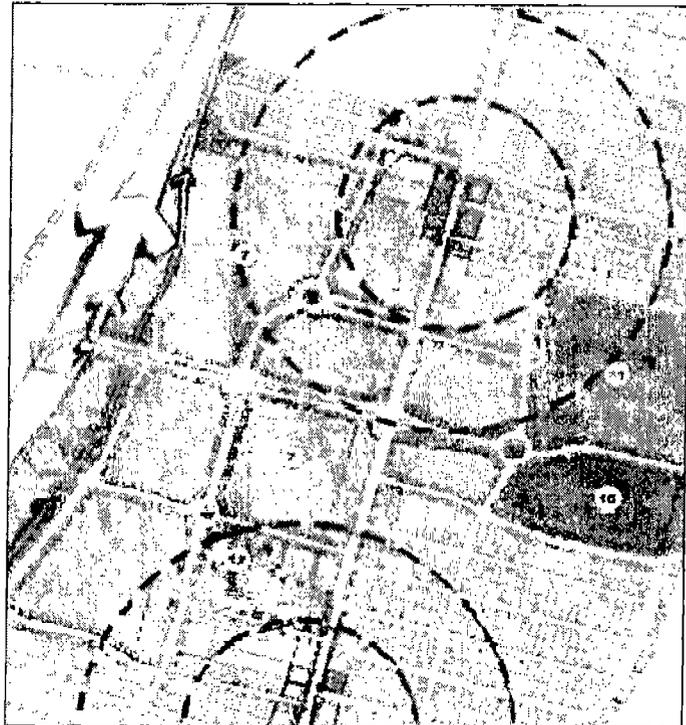
Stanley Avenue looking west toward Highway 33 from Ventura Avenue. Note the visible outdoor storage.

LONG TERM STRATEGY PLAN

This Plan also identifies the significant opportunity at Stanley Avenue to establish a more cohesive strategy to replenish the loss of oil-related uses in the area and provide a more balanced mix of public and private uses. Stanley Avenue enjoys easy access to the freeway and is anchored by a significant corporate user in Kinko's. The proposed strategy acknowledges this facility as a tremendous asset, and seeks to encourage more and similar uses in the proximity. Although this document addresses the Stanley Avenue area at a conceptual level, more focused study of the area's constraints and opportunities are warranted in the near future to help direct both public and private investment.

Consideration should also be given to clarifying the transition of the Westside Community and the Downtown Redevelopment Area, where the two planning areas overlap. Again, there are valuable industrial users in this area which might be better served in another portion of the City, allowing

“This Plan also identifies the significant opportunity at Stanley Avenue to establish a more cohesive strategy to replenish the loss of oil-related uses in the area ...”



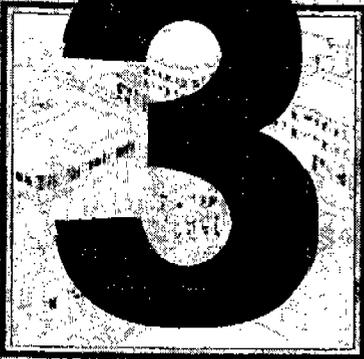
An opportunity to transform the Stanley Avenue area into a high quality business district, supported by easy freeway access and walking distance access to core area services.

this area to be developed into a stronger neighborhood component benefiting both the Westside Community and Downtown.

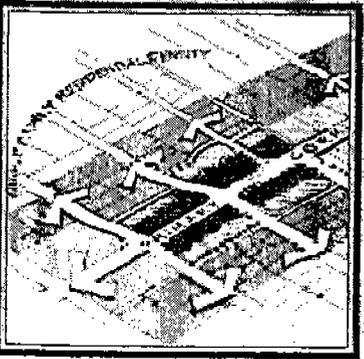
The community participants were clear in their desire not to destroy the historic and very human scale of the Westside Community in the quest for new community investment. The desire is to support development which contributes to the neighborhood qualities of scale and access which already prevail in the Westside. In this way, the plan does not so much introduce new planning concepts, but rather seeks to reinforce the best of what already exists - and to call for more of it where appropriate.



Aerial view of Ventura Avenue at Stanley Avenue, today.



Background



BRIEF HISTORIC OVERVIEW

The Westside is one of the City's oldest and most diverse districts. Before Ventura's beginning with the arrival of Spanish Missionaries in the late 1700's, the area was home to the Chumash Indians, who thrived in this small river valley with its protection of close hills, its proximity to the coast, and near-perfect mild climate.

Soon after the arrival of the Spanish, the abundant resources of this area began to play a crucial role in the establishment of the San Buenaventura Mission and township. Designed by Spanish engineers and built largely by Chumash neophytes, a stone aqueduct was constructed extending several miles up the valley to the confluence of the Ventura River and the San Antonio Creek. Relying solely on gravity, this aqueduct delivered fresh water to the Mission and township, supplying a complex water system that supported domestic and agricultural needs for several decades. The remains of this aqueduct can be found in places along the eastern edge of the valley, including several sites within the boundaries of this planning area.

Although the Mission fell into decline in the early 1800's, the area's rich resources and mild climate continued to attract settlement, both from Mexico and the eastern United States. By the late 1870's, the San Buenaventura Township was well

“Before Ventura's beginning with the arrival of Spanish Missionaries in the late 1700's, the area was home to the Chumash Indians, who thrived in this small river valley, with its protection of close hills, its proximity to the coast, and near-perfect mild climate.”



By the turn of the century, Ventura Avenue was dotted with small farms and houses.

“Ventura’s population more than doubled between 1920 and 1930, from 4,156 to 11,603— an increase of 179%, a growth rate exceeding even that of Los Angeles.”

established, including railroad service and a small port, and the little coastal valley was dotted with small farms. Growth within the community remained focused around the town’s geographic center at the Mission until the turn of the century, when pressure began to edge the community gradually eastward.

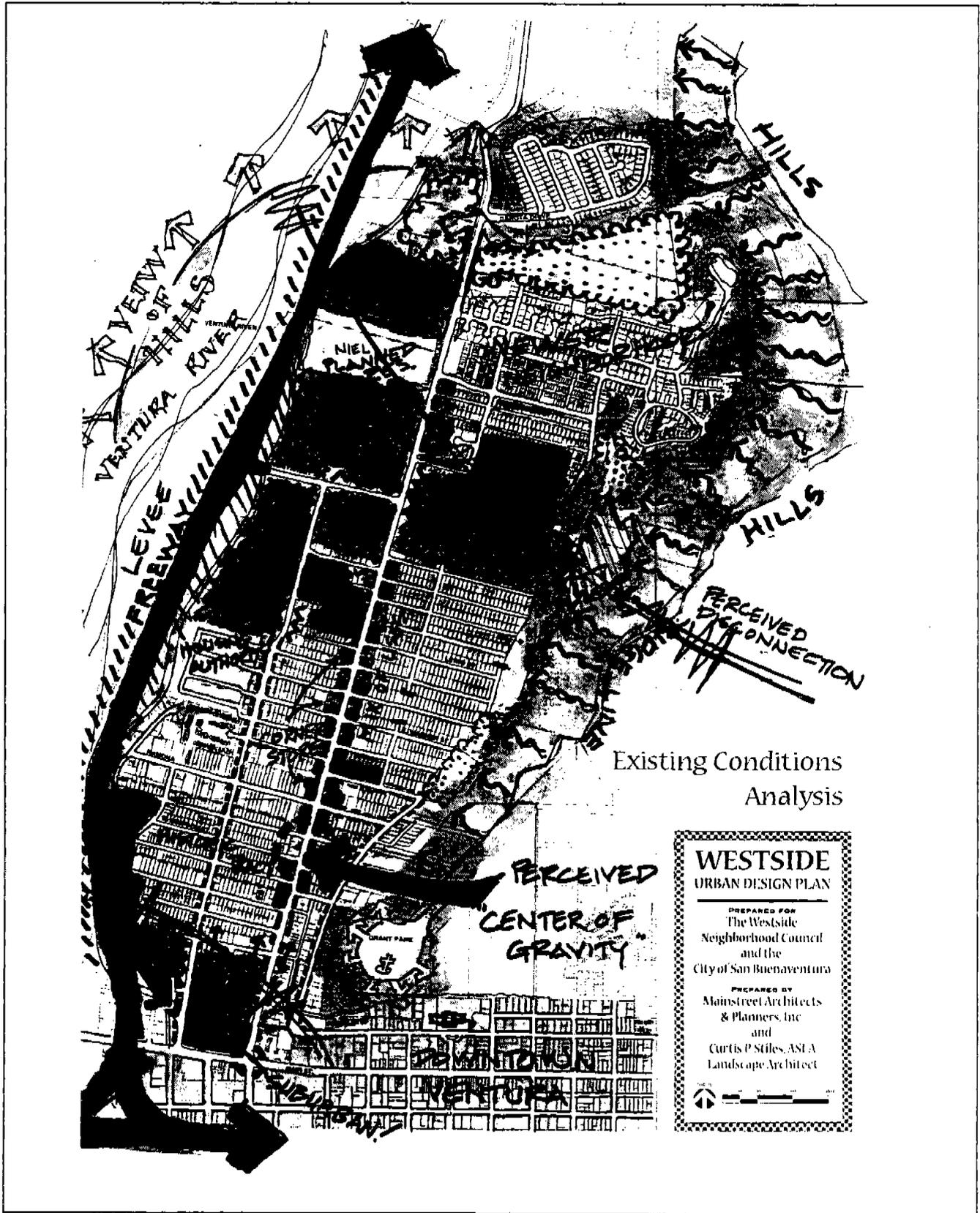
Black Gold

Change came rapidly for the Westside, when in the early 1910’s substantial oil reserves were discovered. The City’s population more than doubled between 1920 and 1930, from 4,156 to 11,603 – an increase of 179 percent. The Westside, then known fondly as the Avenue, became dotted with small subdivisions, mostly working class bungalows and cottages and numerous oil-related businesses which sprang up along its length. The rest of the City felt the pressure, and the benefits, of this economic boom which pushed the City’s boundaries further Eastward and up into the hills above the town.

Although it took a mere decade for the area to become substantially built-out, for the next fifty years the Avenue continued to play a significant role in the economic growth of the City. As a compact, well-defined geographic area, it has supported a fairly intense mix of uses, including residential, commercial, manufacturing and both light and heavy industrial. Most of what exists today was constructed 25 to 50 years



Fueled by the discovery of substantial oil reserves beneath the Westside, Ventura’s population expanded rapidly.



Graphic analysis of some of the issues to which the Westside Urban Design Plan must respond.

"... the community saw a substantial drop in oil-related industry and the beginning of its exodus. Although many oil-related businesses still remain, there has been substantial decline in oil-related jobs and investment in the region."



Boundaries of the Westside Plan area. Note the overlap with the Downtown Specific Plan.

ago. The most significant exception is the VETCO Corporate facility (now KINKO's), a steel and glass complex which was constructed in the early 1980's by the local oil tool company. Not long after, the community saw a substantial drop in oil-related industry and the beginning of its exodus. Although many oil-related businesses still remain, there has been substantial decline in oil-related jobs and investment in the region.

EXISTING CONDITIONS

District Boundaries

The Westside Community District, as addressed in this plan, is bounded by the Ojai Freeway (Hwy 33) to the West, by the hills and Grant Park to the East, by the "Dakota Tract" to the North and by Main Street to the South. Note that the southern boundary overlaps with the northerly boundary of the Downtown Specific Plan. Although it is currently within the boundaries of the Downtown Redevelopment Area, there is clearly need to address this area as integral to the success of the Westside revitalization efforts. As these efforts begin to formalize, some boundary adjustments may be appropriate in this regard.

There are significant parcels of land held by oil-related industries in the northern portion of the Westside area, much of this outside the City's boundaries. Although concern for soil contamination is real, these parcels can likely play a significant and strategic role in the Westside's and the City's business and development future.

Access and Circulation

The geography of the district is strongly linear, defined by physical barriers of raised highway on the East and steep hills on the West. Because of this, access is limited to two parallel primary streets – Ventura Avenue and Olive Street – at both their northerly and southerly ends. These two thoroughfares serve residents as well as heavy truck and commercial access throughout the district.

Ventura Avenue also supports a significant amount of through traffic, serving as a primary transit corridor for West Ventura and the Ojai Valley. On the South, there is also a third point of access at Cedar Street, which primarily serves local residential users as a link to Downtown and parts East.

In addition, the City's Comprehensive Plan identifies an intended network of bicycle lanes and paths, with three classes of physical definition. These include "class three bike routes," which are designated by occasional signs, a "class two bike lane" designated by striping, and a "class one bike path" which is physically separated from other types of traffic.

At present, Ventura Avenue is designated as a "class two bike lane" in the Comprehensive Plan. Although striping is not consistently present, the lane is used frequently by bicyclists, despite the obvious risks of heavy vehicular traffic. Currently under construction is a bike trail that will link West Ventura with the Ojai Valley trail system and the beach. This trail affords an opportunity to enhance and improve the bicycle network in the Westside Community.

Opportunities and Constraints

In many ways the physical constraints of the Westside also represent much of its opportunity. The existence of clear physical boundaries means that new outward growth is very limited. Yet this means that in response to public demand and seed investment, economic pressure will likely result in new urban in-fill development as envisioned in this plan.

Ventura Avenue

This is clearly the most significant connector on the Westside. Currently considered "substandard" in width, older parts of Ventura Avenue lacked full improvement of curbs and sidewalks until very recently. When City engineers first proposed a significant improvement package, including widening of the street section, residents came en masse to speak in opposition, fearing that wider streets would mean loss of many older buildings and an increase in traffic quantities. In response, the plans were revised to conform to the narrower existing section.

The constraint that the existing Ventura Avenue cross section represents is very crucial to the area's future. With a "cap" on capacity, alternative methods of access become more and more viable as demand increases. Public transit, bicycle and pedestrian access will all increase in demand as the Westside begins to reemerge as a viable in-fill area. Furthermore, the prerequisite urban environment required for



Currently Ventura Avenue is the primary corridor for all manner of transportation, despite some risk.



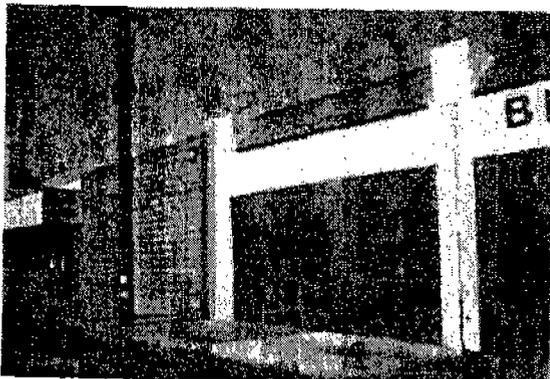
The Westside's historic development pattern evolved into a series of small commercial "centers" spaced within walking distance of the district's various neighborhoods.



The corner grocery was a typical focus for neighborhoods in the Westside.



Many of the Westside's existing neighborhoods boast the architectural character of mediterranean bungalows that were so popular in Southern California in the 1920s.



This is not an abandoned building! It's actually a thriving business. Property owners often do not realize the impacts appearance has on people's perceptions about safety and property value.



This mixed-use commercial/residential building is at the heart of what can become a strong pedestrian core.

a truly walkable district would be substantially undermined if Ventura Avenue were widened significantly.

It is interesting to note that the historic development pattern along Ventura Avenue – small commercial “centers” within walking distance of surrounding residences, and linked by stretches of business or residential blocks between – is essentially where this plan has arrived as a result of the public workshop process.

Existing Neighborhoods

The Westside Community is made up of several distinct neighborhoods, each with unique qualities and attributes. Most of the neighborhoods to the south of Stanley Avenue were constructed during the 1920's oil boom. One of these, the Simpson Tract, is a City of Ventura Historic District, so designated for its representation of a 1920's working class neighborhood composed of cottage and craftsman style bungalows.

A few blocks to the north of Stanley Avenue the neighborhoods represent much more recent development, with a decidedly more suburban character. This area, which includes the “Brock Homes” development and the “Dakota Tract,” is both physically and perceptually separated from the majority of the Westside Community, with the intersection of Ventura and Stanley Avenues being the only current point of connection. Separated by a number of industrial uses and confronted with the associated truck traffic, the tendency of these residents is to by-pass Ventura Avenue, using the Stanley Avenue freeway connection for access to the balance of community services downtown and further East.

Alleys

There are a number of alleys, many of which serve as small streets, which link portions of surrounding neighborhoods to Ventura Avenue (some of these have been abandoned or blocked off). Depending upon the point of view, such alleys can be an asset or a nuisance. Several of the Westside's existing alleys are well kept and well used by residents, both for vehicular and pedestrian access. Alleys are served well when there are secondary residential units that gain access from them and can enjoy visual observation from windows and porches (“eyes to the street”).

Mixed-Use

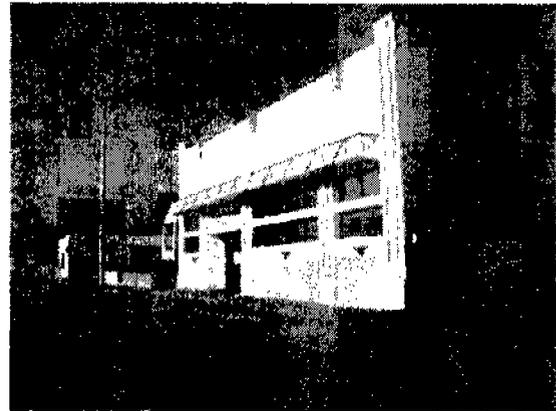
There are a number of existing buildings which provide a mix of residential and commercial uses. The opportunity to bolster these and in-fill with new mixed-use buildings of a similar nature will provide a strengthened customer base for local commercial uses, as well as new customers within easy access of transit lines.

Industrial Uses

The Westside Community is home to numerous industrial businesses, many of which served the oil industry and which have remained viable even in the face of a regional industry downturn. There are some significant issues of concern regarding adjacencies of residential and certain heavy industrial uses. In addition, many sites contain unscreened outdoor storage of vehicles and equipment, much of it occupying prime property frontage.



As a main entry into the Westside Community, the appearance of Stanley Avenue has a tremendous impact upon people's perceptions about the area.



This business recently renovated their building front in a manner suggestive of the architecture of the 1920's and 1930's.



This exceptional 1920's period design waits for a business to breathe new life into it. Until recently, it was used as a warehouse for a tire company, but it is said that it was once home to the Library.



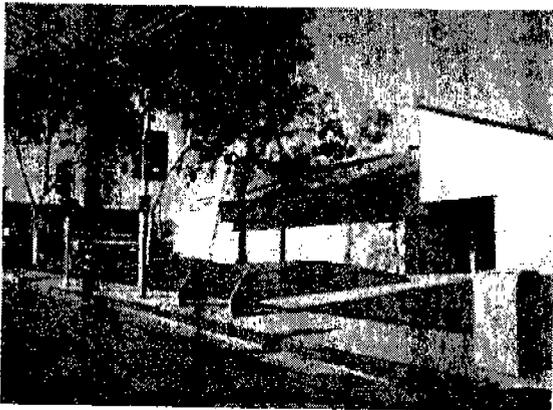
The Westside is home to many long-time businesses. Avenue Hardware is known for its personal service and awesome inventory!



The Westside Community is home to numerous industrial businesses, many of which served the oil industry and which have remained viable even in the face of a regional industry downturn.



Among the Westside's most famous home grown businesses is "Johnny's Mexican Food." The line for Johnny's burritos starts forming around 11:30 am. (and it's in a mixed-use building too!).



The Avenue Adult Center provides many services to the Westside Community, including classes, meeting space, and the opportunity to connect with others.

Auto-related Uses

There are a number of auto-related uses, primarily repair-related activities and used car sales. Many of these front on Ventura Avenue with little or no visual screening of stored cars and equipment. Several are adjacent to residential uses.

Transitional Areas

Between Main Street and Park Row, the southerly portion of the project area overlaps with the Downtown Specific Plan Area. At present, this area includes industrial uses that are important commercial assets for the community. However, in their current location the result is that the Westside neighborhood to the immediate north is separated and "cut off" from the adjacent neighborhood commercial center by an industrial "no man's land" which is nearly deserted at night and on weekends.

Public Facilities

The Westside is served by some excellent public facilities, including the Avenue Adult Center where many classes and activities are offered, as well as two small parks, two elementary schools and one middle school. In addition to these, the Westside citizens have created a small public meeting park on a private parcel adjacent to the Adult Center.

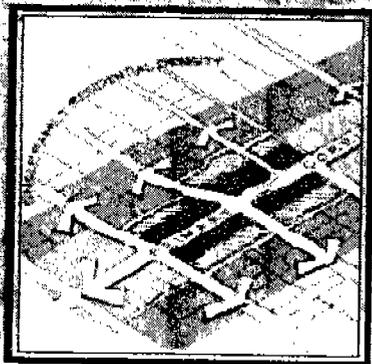
Soon to move to a new and slightly larger facility in the Historic Casa de Anza building, the Avenue Community Library provides branch services to the Westside Community.

"The Westside is served by some excellent public facilities, including the Avenue Adult Center, where many classes and activities are offered..."



4

The Public Realm



THE PUBLIC REALM OF THE WESTSIDE

The design of public improvements within the Westside District addresses a hierarchy of importance and includes both primary and secondary design elements as described below:

Streets

Streets, for the most part, make up the majority of what we commonly call the "Public Realm" of the Westside. Streets are where we shop, walk, drive, sightsee, and where we meet one another outside of our private domains. Streets belong to all of us in the same way as public parks and plazas.

"Streets are where we shop, walk, drive, sightsee, and where we meet one another outside of our private domains. Streets belong to all of us in the same way as public parks and plazas."



The improvements of the public realm are not meant to "reinvent the Westside," but to help complete it.



Ventura Avenue as it appears today.



Ventura Avenue with public improvements complete.



Ventura Avenue with public improvements and commercial renovations complete.

Essential Ingredients of Successful Streets

Streets can be a source of public pride or public shame. Street environments can have almost as much effect on people's lives as the quality of their own homes. Streets are settings for activities that bring diverse groups of people together in sometimes random and unplanned ways and sometimes organized and structured ways.

Accessibility

Streets are essential parts of our living space. As such they must function well, but they must also be aesthetically pleasant. Streets should ease the tensions between pedestrians and vehicular traffic, making accessibility and movement equally comfortable for both. Streets should be easy to identify and easily accessible.

Scale

There are many ways of achieving a pleasant street, but one essential ingredient is that all elements should be on a human scale and related in character. Buildings, trees, lights and street furniture are common elements of the streetscape which form the edge of the public space and establish the scale of that space.

Safety

People should not have to worry about being hit by a car or truck on a public street. People should not have to worry about stumbling on paving or bumping into street signs, street furniture or light standards which are placed in unpredictable locations. Lighting levels are important, but too much light can be as distracting and as much a danger as too little light.

"Eyes on the Street," a precept advocated by Jane Jacobs in her book "The Death and Life of Great American Cities" argues that the most secure streets are those that have many eyes looking onto the public realm. Window and door openings facing outward from occupied buildings aid in creating safe streetscapes.

Design Hierarchy

A discernible hierarchy of elements along the street's length orients us to the uses and activities of that street. Streets

should be representative of the district in which they are found. Streets that move through different types of districts must reflect the uses and diversity along its edges, but should contain some common element that people remember and connect to that particular street.

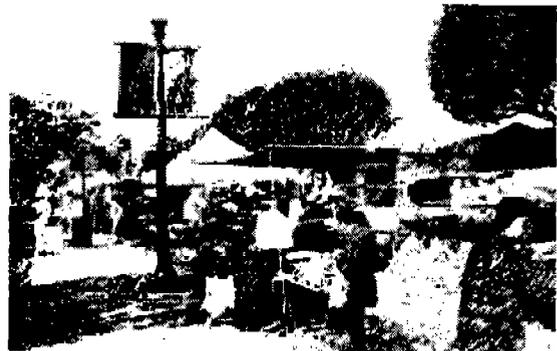
As an example, lighting elements may share the same light poles, but different districts may have multiple globes or armatures for banners to let us know that this is a district with more intense public functions. Street trees may remain the same species, but districts may have different spacing or may have tree grates at their base rather than simple tree wells. Street entourage (bus benches, waste containers, lighting, signage, etc.) can help tell us where we are and what kind of activities we can expect to participate in while we're there on a particular street.



Signage, decorative street lights, and street trees.



Street trees and decorative street lights with banners



Plaza activities



Residential planted parkway.

Residential Street Improvements

Residential streets become more serene, more beautiful and more valuable when they are lined with closely spaced shade trees. Trees lining a residential street create an atmosphere in which most people would prefer to live.

Most streets within the Westside do not have street trees. Participants of the workshops almost unanimously agreed that all residential streets within the Westside Community would benefit aesthetically and ecologically from the planting of street trees. The species should be carefully selected to allow shade canopy in summer, warming sun in winter and wherever possible these trees should have a non-intrusive root structure.

Landscape of Residential Streets

Residential Streets which do not have street trees should be planted with trees that have been successful in the Ventura area, and trees that have a good chance of survival without continuous, intensive maintenance.

Partial List of Suggested Trees

- African Sumac Tree
- Bailey's Acacia
- Firewheel Tree
- Orchid Tree
- Carrotwood Tree
- Brisbane Box Tree
- Aristocrat Pear Tree
- Evergreen Elm Tree
- Crepe Myrtle Tree
- Holly Oak Tree
- Liquidambar Tree
- Jacaranda Tree
- London Plane Tree
- Metrosideros Tree
- Queen Palm Tree
- Honey Locust Tree
- Tulip Tree
- Eastern or Western Redbud Tree
- Purple Leaf Plum Tree



Existing Bottlebrush.

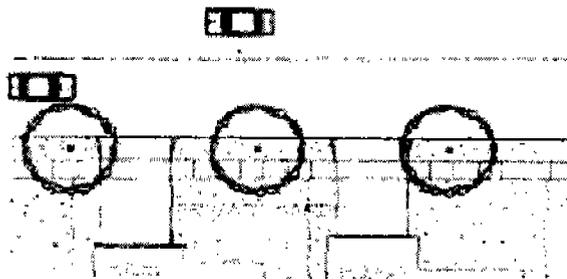
Residential Street Tree Locations

Trees should be planted in parkway strips adjacent to the curb where parkways are existing.

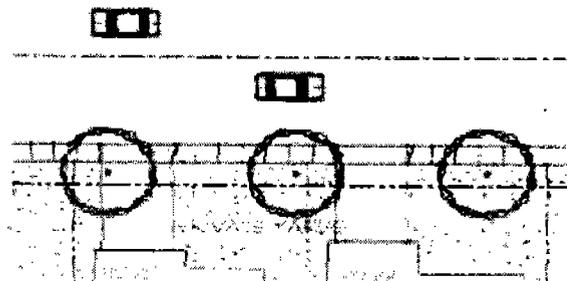
Many of the Westside's residential streets do not have adequate room for planting of street trees within the public parkways. In such case trees should be planted in private yards in a landscape easement or should be planted in small planters located within the street parking zones. If possible, it is desirable that trees be spaced so that there is at least one tree for every lot and that the spacing not exceed thirty five feet on center.



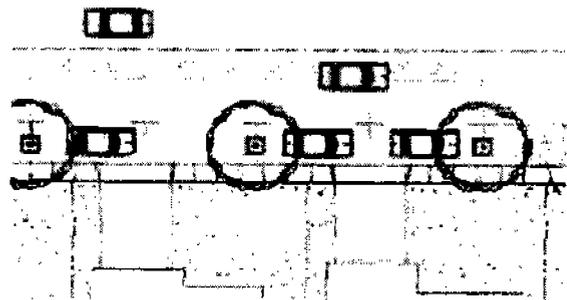
A gateway into a residential area.



ALTERNATIVE A
35' ON CENTER



ALTERNATIVE B
35' ON CENTER



ALTERNATIVE C
35' ON CENTER

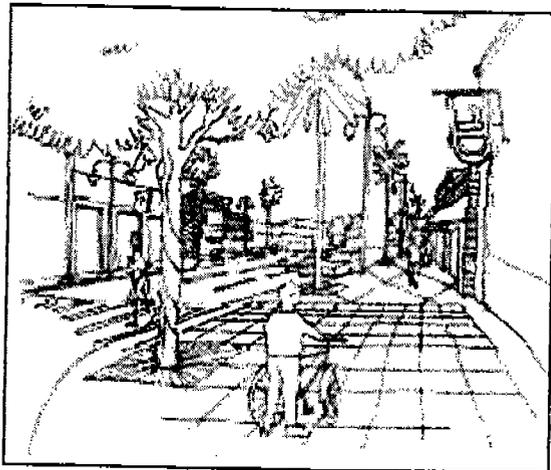


Alternative Residential Street Sections



Hierarchy of trees, street lights and banners.

“Shopping can be agreeable in a district which is well served by public/private transport and has thoughtfully placed trees, seating, lighting, and other amenities.”



Core areas will boast canopy trees at the corners, with vertical trees and street lighting punctuating the length of the block.

Retail/Commercial Street Improvements

Memorable streets are places where we want to spend our time. Why? Certainly the best streets encourage participation by all. The best streets are places where we want to be; to spend time watching, playing, working and being together.

So, is this simply a result of chance, or can we design streets that are destined to be successful places for people on foot or on bicycles, while also serving as efficient transportation modes for automobiles and trucks?

Shopping can be agreeable in a district which is well served by public/private transport and has thoughtfully-placed trees, seating, lighting, or other amenities; or it can be a nightmare of dragging children through overcrowded parking areas while dodging automobiles and trucks.

Street Improvements for the Westside

Ventura Avenue, Stanley Avenue and to a lesser extent, Olive Street, constitute the Commercial/Retail/Industrial streets of the Westside. At present they serve residential commuters, heavy industrial trucking, and service vehicles simultaneously sharing space with bicyclers, walkers and skateboarders.

Ventura Avenue

Ventura Avenue is the main thoroughfare within the district, and serves as a connector to other areas of the community and also defines the main retail corridor. Historically Ventura Avenue has been the key artery serving all other commercial and residential areas of the Westside. This street accommodates relatively high volumes of vehicular and pedestrian traffic. Careful design can assure that it does both comfortably.

Typical Street Section for Ventura Avenue

Two 11 foot travel lanes, one 11 foot turning lane, and two parking lanes of 8.5 feet and sidewalks of eight feet on both sides of the street constitutes the typical street section and functions well for automobile and pedestrian traffic. Improvements for the Typical Street Section include:

Sidewalk Enhanced Paving

Scored concrete with diagonal score pattern and broom finished.

Crosswalk Paving

Interlocking concrete pavers or colored scored concrete.

Lighting

Simple, attractive and durable pole lights which identify the district as a place of safety, but are not trendy elements which are costly and become outdated within a short period of time.

Landscaping

Street tree landscaping should be uniform in appearance with a single variety of trees except at the Core Area intersections where flowering accent trees should be planted at each corner of the intersection. Street trees should be placed in sidewalk tree wells adjacent to the curb and should be placed not more than forty feet on center and not less than thirty five feet on center. Street trees should be viewed not only as single landscape elements but as *Vertical Edges to the Public Space*. Unity of specie type and size become important elements in creating the street as the public realm.

Possible Ventura Avenue Street Tree Species

- | | |
|-------------|-----------------------------|
| King Palms | Thornless Honey Locusts |
| Queen Palms | Bradford "Aristocrat Pears" |

Possible Accent Trees at Intersections

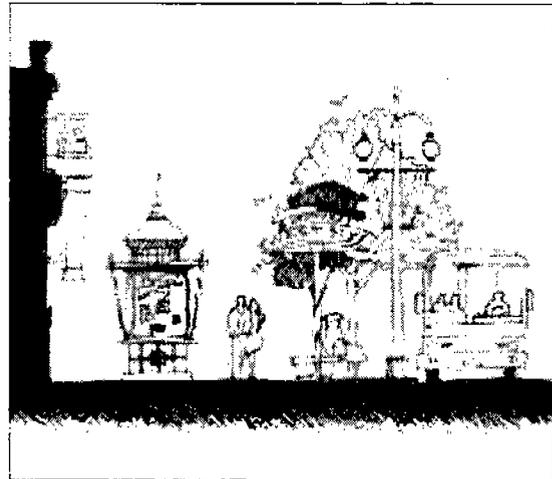
- | | |
|------------------|----------------------|
| Jacarandas | Crepe Myrtles |
| Silk Floss Trees | Gold Medallion Trees |

Street Furniture

Powder-coated/slatted steel benches, waste containers, and handicapped accessible drinking fountains should be durable and easily maintained, but should not have an institutional appearance. Colors may range from black to bright primary hues, but should be unique to the Westside.

Street Signage

Street identification signs should be coordinated with the normal city standard in terms of form and location, but graphics and font styles should be unique to the Westside. Local artisans could be solicited for participation in the design and construction of signage and graphic elements.



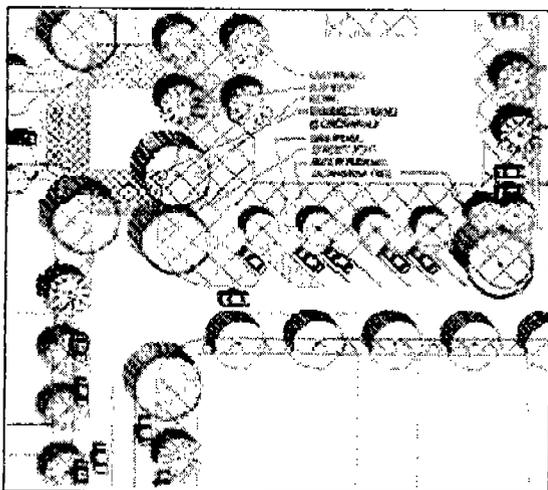
Street furnishings, including kiosks, bus shelters, street lighting, etc. would enrich the public realm within the core areas.



Street scene at a mini-plaza.



Street scene at core area featuring an informational kiosk.



There will be opportunities for "mini-plazas" to be created on corners of key intersections.

Pedestrian Core Areas

Pedestrian Cores are for people - a place where the automobile is not the predominant factor. They are to be located in three sections of Ventura Avenue. These areas are to have intensified streetscape improvements to make the area more friendly to diversified pedestrian activities while allowing slower but still efficient automobile traffic.

The Pedestrian Cores are located along the length of Ventura Avenue, as follows:

- Fix Way north to Ramona Street (Primary)
- Flint Street north to Lewis Street (Secondary)
- Shoshone Street north to Seneca Drive (Secondary)

Pedestrian Core Improvements

Sidewalks

Core Areas should have a re-configured street section which features enhanced sidewalks. "Traffic Chokers" or "Curb Extensions" should be constructed at intersections. These Traffic Chokers should narrow the streets by 20 feet and allow two 11-foot travel lanes of through traffic with an 11-foot wide left turn pocket. No parking should be permitted at the curb of the Traffic Choker. These curb extensions are intended to narrow the automobile travel lanes making pedestrian crossings easier and safer. Sidewalks are to be concrete with diagonal score lines at no less than 4 feet on center in each direction.

Crosswalks

Crosswalks should be paved in colored, scored concrete with a modular, diagonal pattern of score lines or sawcuts. The concrete surface should also have a roughened texture conducive to safe walking during inclement weather conditions.

Street Trees

Sidewalks should be planted with street trees approximately 35 feet on center in cast iron tree grates adjacent to the curb line. Tree grates should be 42 inches square and should be a design unique to the Pedestrian Core Areas.

Mini-Plazas

A by-product of the Core Area "traffic choker" is the creation of "mini-plazas" or enlarged paved areas at intersections.

Mini plazas are to have enhanced paving and are to be planted with accent trees which will identify the Pedestrian Core for both pedestrians and motorists.

In addition, the mini-plazas should contain information kiosks, trash containers and benches for pedestrians. The City should encourage participation of local artisans to participate in the design and construction of these elements. Additionally planter pots may be located in the public right-of-way at business facades. These planters may contain colorful perennial and annual plantings. Planted pots should be a joint effort of the City and the business owners in terms of their purchase, planting and maintenance.

The mini-plaza also creates a widened area on the residential streets that run perpendicular to Ventura Avenue. This widened area is sufficient to provide enough width for diagonal parking. This is particularly helpful at the Avenue Library on Simpson Street and the Avenue Adult Center on Center Street.

Enhanced Bus Stop Shelter Areas

Enhanced Bus Stops should be capable of holding more than a dozen people and should have a roofed structure to shelter people from both sunshine and inclement weather. The shelter should also be well lighted to identify their location and to provide a safe haven for potential riders.

Lighting

"Special District Pole Lighting" festooned with colorful banners will line and define the edge of the public street. These lights should be no more than fifteen feet in height and should be placed no further apart than forty feet. Ideally the lighting will be centered between the Street Trees.



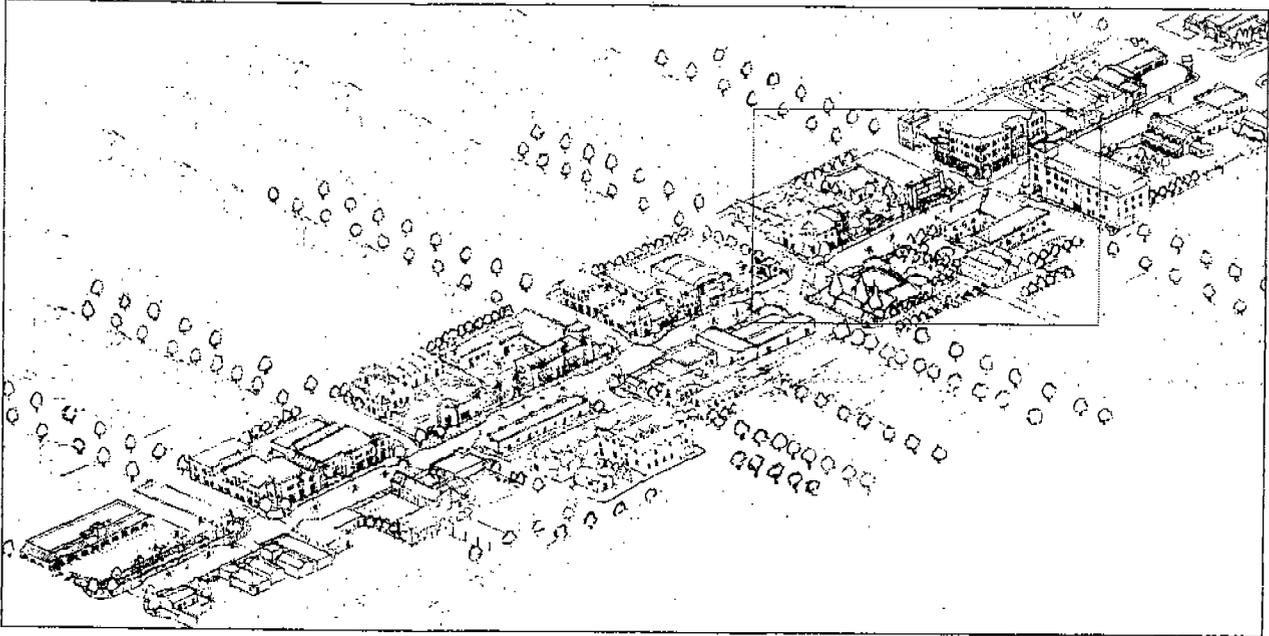
Street scene with parallel parking.



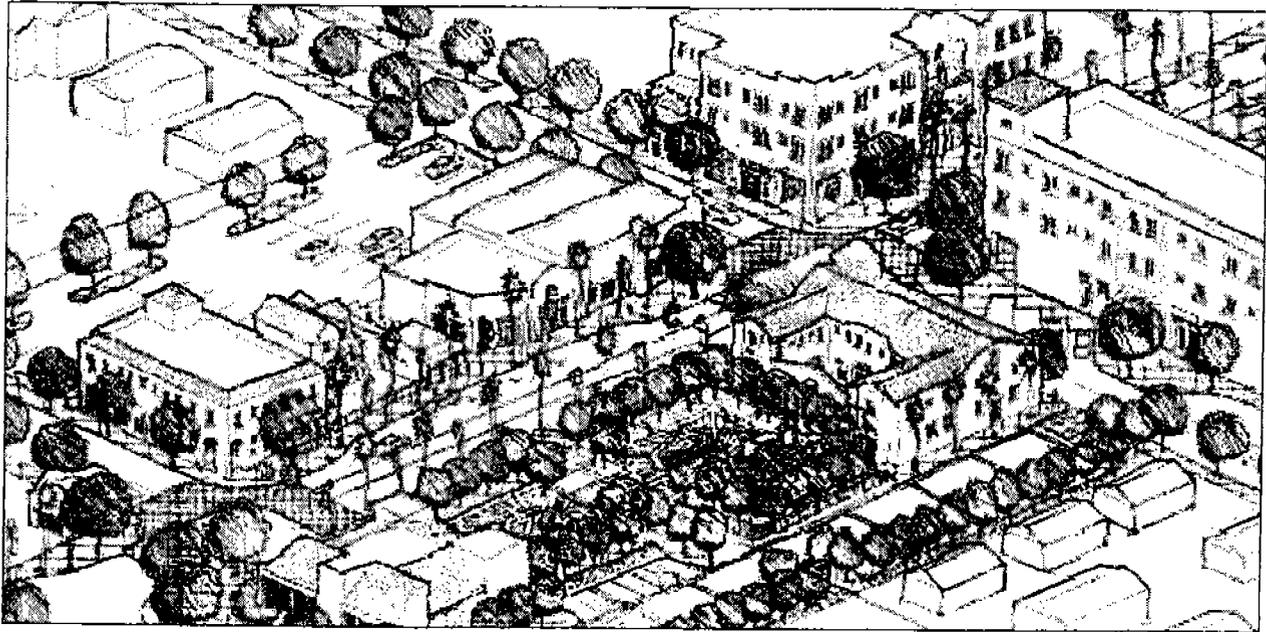
Bus shelter.



There is a wide variety of lighting standard types.



View from above, looking at the five block length of the Westside's Primary Pedestrian Core.



A close-up view from above depicts the conceptual design for the Plaza de Avenida - the Westside's public square.

Connecting Areas

These Interstitial Zones have a street section with two 11 foot travel lanes, one 11 foot passing lane, and two parking lanes of 8.5 feet and sidewalks of eight feet on both sides of the street. This replicates the existing Ventura Avenue street geometry that functions well for automobile and pedestrian traffic.

Since the Interstitial Zone is defined as less intense, the streetscape improvements will be detailed in a simpler manner but will still contain pedestrian-friendly features.

Connecting Area Improvements

Sidewalk Paving

New concrete sidewalks with a simple score pattern and a medium broom finish for safe, comfortable walking.

Crosswalk Paving

Interlocking concrete pavers or colored scored concrete.

Lighting

Pole lights should be simple, but attractive and durable. The lights should identify the district as a place of safety, but not be trendy elements, which are costly and become outdated within a short period of time. These lights should be no more than twenty feet in height and should be placed no further apart than fifty feet.

Street Trees

Street trees should be spaced 35 to 40 feet on center. Trees should be placed in sidewalk tree wells adjacent to the curb. Tree wells should be 42 inches square. Street trees should be of the same species planted in the "Core Pedestrian Zones".

Street Furniture

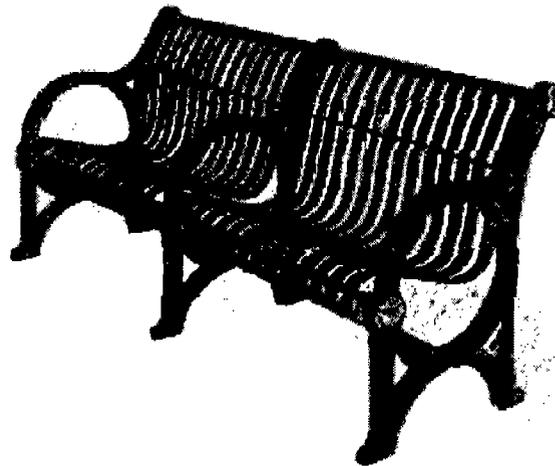
Powder-coated, slatted steel benches, waste containers, and handicapped accessible drinking fountains.

Street Signage

Street identification signs should be coordinated with the normal City standard. Projecting signs should be encouraged as long as they are in substantial compliance with citywide standards and guidelines.



Enriched paving at crosswalks



Street furniture should be durable, comfortable and attractive.

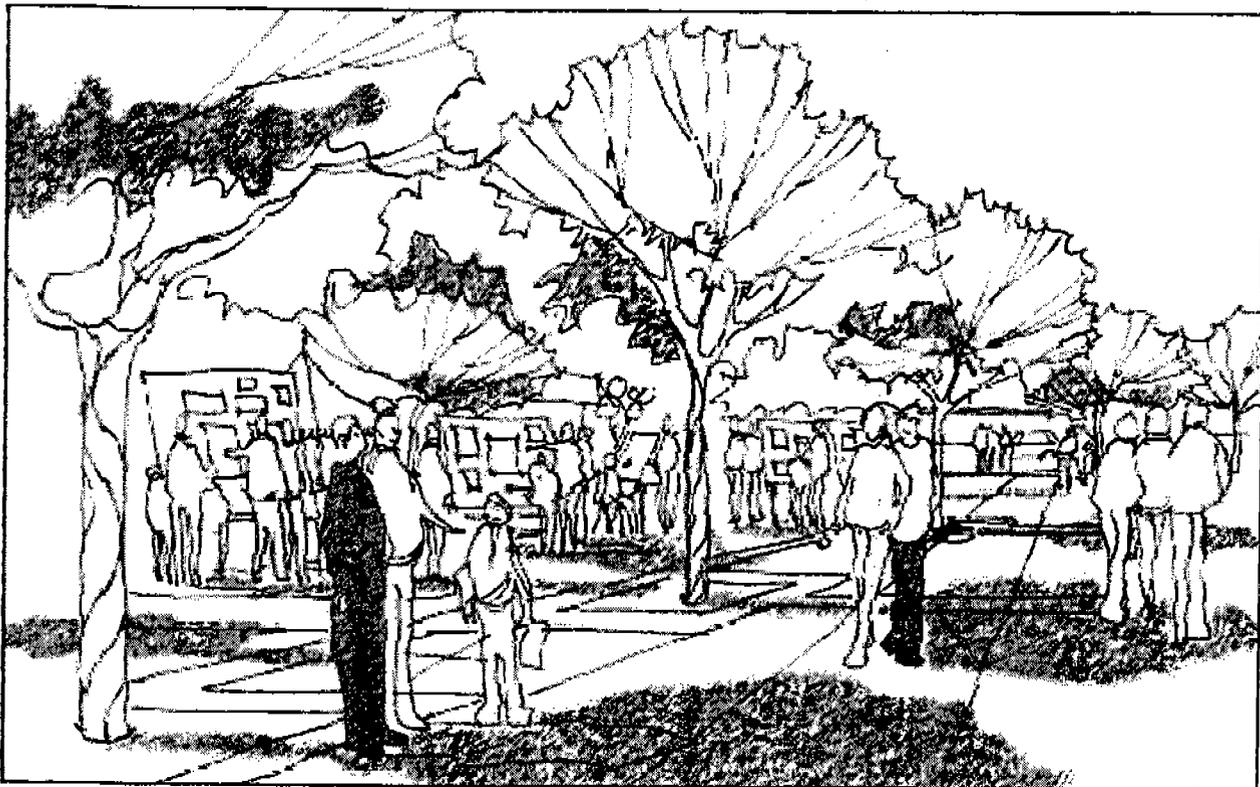
Plaza De La Avenida

At the perceived mid-point of the primary pedestrian core stretching from Fix Way to Ramona Street, a traditional plaza provides a public gathering space for both active and passive recreation. Street fairs, farmers market, dances, and concerts can be held on weekends, holidays, and other appropriate times. Sitting, meeting, chess or other board games may constitute normal daily activities. This plan acknowledges the importance of this community space and seeks to assure its permanence.

"...a traditional plaza provides a public gathering space for both active and passive recreation. Street fairs, farmers market, dances, and concerts can be held on weekends, holidays..."

Features

The Plaza as it exists today is a delightful, very informal space, hand-hewn from found and gathered materials. It was constructed by community members on private property, with the objective of providing a gathering place for events and celebrations. This plaza should be made a permanent asset of the community, either at its present location or a similarly



The Plaza de la Avenida provides an urban space suitable for community wide cultural events, music and art events.

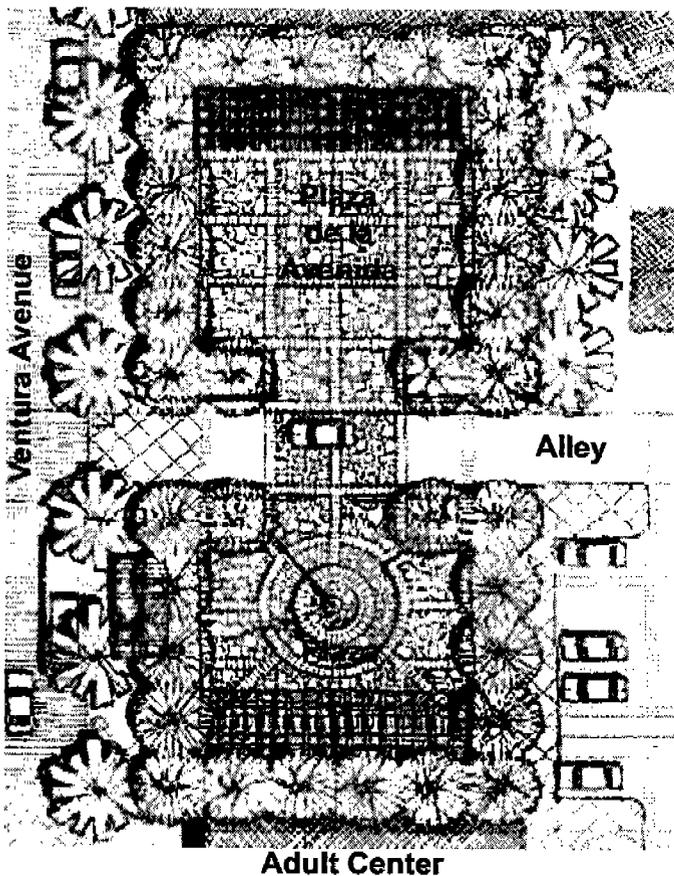
suited site. This document includes some design features and recommendations which could be considered for future improvement. However, if the plaza is retained at this site and altered, or if a new site is selected, the alterations or design should be based upon input from the community through a fully-involved workshop process.

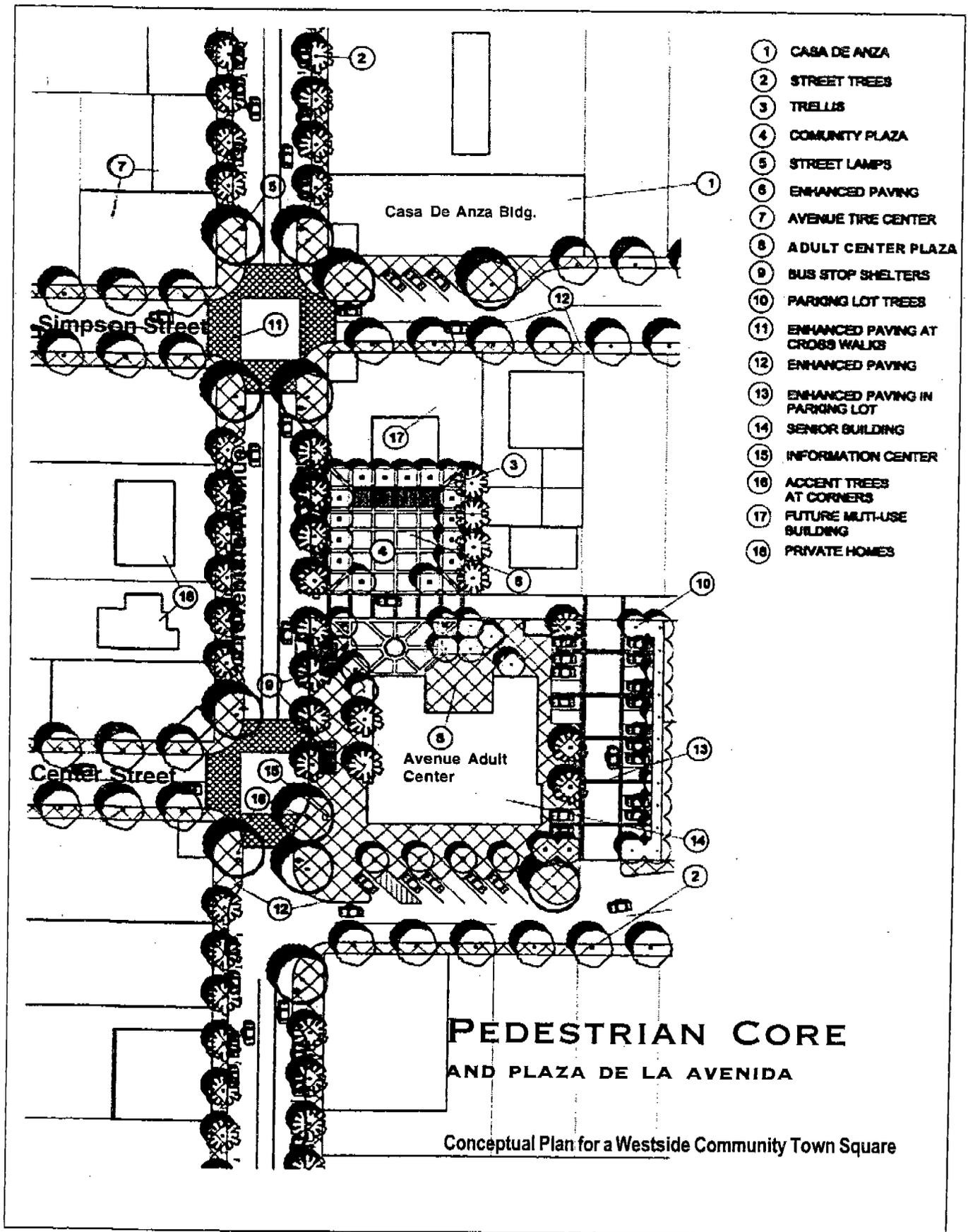
As shown here, the plaza features enhanced paving, such as brick or stone, and could be landscaped with a Tree Bosque on its outer edge and an arbor/colonnade for shelter. The center of the plaza should remain a people space with as much flexibility as possible for gatherings, speeches, concerts and other such events. Public art and amenities should be a part of the plaza and the participation of local artisans should be included in any improvement or reconstruction of the plaza. The following graphics are intended as conceptual suggestions only.



Plaza entourage.

"The center of the plaza should be a people space with as much flexibility as possible for gatherings, speeches, concerts and other such events. Public art and amenities should be a part of the plaza and the participation of local artisans should be included in both the design and construction of the plaza."





- ① CASA DE ANZA
- ② STREET TREES
- ③ TRELLIS
- ④ COMMUNITY PLAZA
- ⑤ STREET LAMPS
- ⑥ ENHANCED PAVING
- ⑦ AVENUE TIRE CENTER
- ⑧ ADULT CENTER PLAZA
- ⑨ BUS STOP SHELTERS
- ⑩ PARKING LOT TREES
- ⑪ ENHANCED PAVING AT CROSS WALKS
- ⑫ ENHANCED PAVING
- ⑬ ENHANCED PAVING IN PARKING LOT
- ⑭ SENIOR BUILDING
- ⑮ INFORMATION CENTER
- ⑯ ACCENT TREES AT CORNERS
- ⑰ FUTURE MULTI-USE BUILDING
- ⑱ PRIVATE HOMES

**PEDESTRIAN CORE
AND PLAZA DE LA AVENIDA**

Conceptual Plan for a Westside Community Town Square

Bicycle Trails

Bicycle trails should be integrated into the Westside community to serve as transportation corridors and as recreational facilities .



Section at Cedar Bike Trail with retaining wall.

The Ventura River Trail on the West Side of the community and the Cedar Street Extension Trail on the East Side of the community will serve as bicycle arterials. Access pathways from residential neighborhoods should be located at Shoshone Street, Stanley Avenue, Vince Street, and Prospect Street at Westpark and West Park Row.

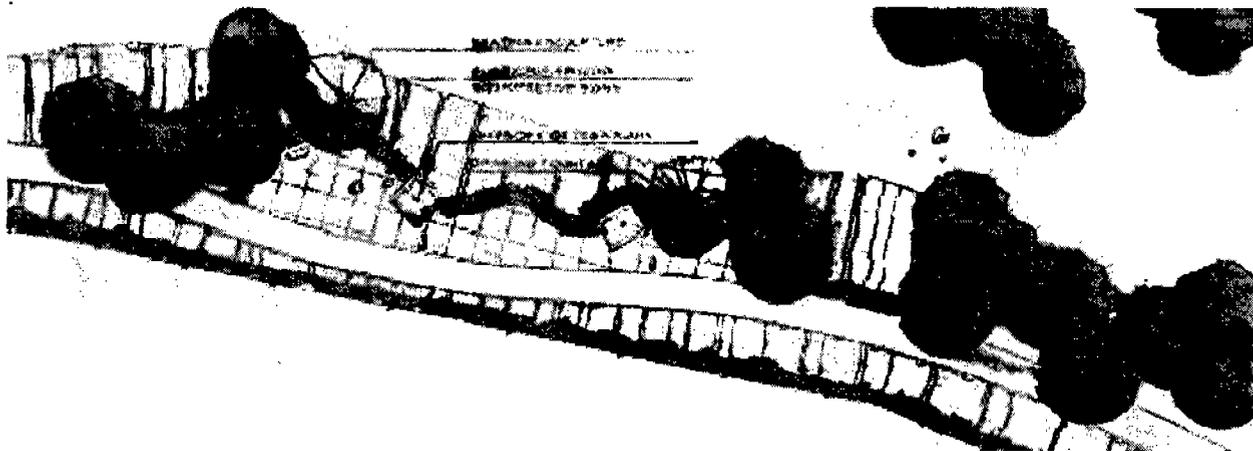
The Ventura River Trail

This trail extends 7.5 miles from Foster Park to Main Street and is currently under construction. The trail will accommodate recreational walkers, runners and skaters as well as bicyclers.

Cedar Street Bicycle Trail

Cedar Street should be extended as a bicycle trail serving neighborhoods east of Ventura Avenue thereby reducing the amount of bicyclist trips on the Avenue.

The Cedar Trail should be a sixteen-foot wide trail centered in an approximate 30-foot right-of way. This trail should be landscaped with Eucalyptus or Poplar trees planted at the edge of the right-of-way approximately twenty-five feet on center. Tree hedgerows along the sides of the trail will provide easy identification of its location and eliminate the need for other maintenance intensive groundcovers.



A bicycle trail rest area.

Gateways

Gateways should introduce the Westside District to visitors and give them directional insight into what they can expect to encounter in the neighborhood. Gateways serve as thresholds to outsiders and symbols of community pride.

Gateways for the Westside should be limited to monumentation within the current Ventura Avenue right-of-way unless acquisition of some private property can be negotiated.

Gateway Locations

Gateways should be located at the intersection of Ventura Avenue and Main Street and at the intersection of Ventura Avenue and Stanley Avenue. Lesser gateways may be located at Olive Street and Stanley Avenue and at the intersection of Olive Street and Main Street. The Pedestrian Core Areas also serve as gateways within the community.

Gateway Features

Gateway features should consist of masonry features and decorative sign walls at the edge of the right-of-way. The intersection paving should be colored bowmanite or exposed aggregate concrete. Gateway monumentation should also contain Art in Public Places with local artisans involved with both design and installation of public art. Gateways should be augmented by specimen size Palms or Jacaranda trees and accent lighting should illuminate trees and monumentation.



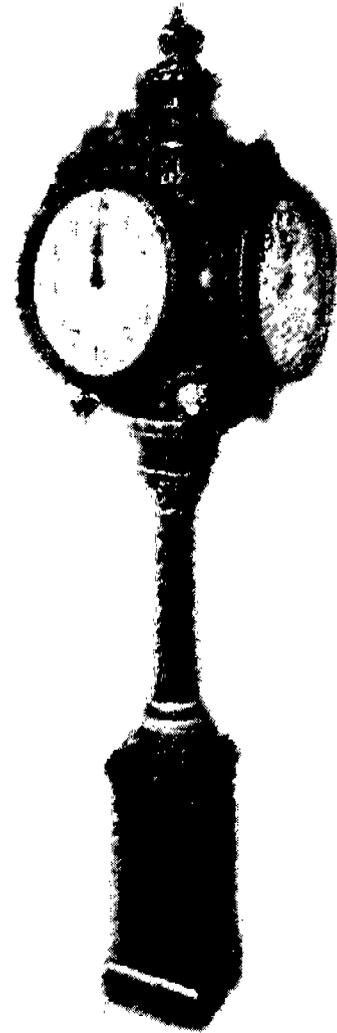
Formal design elements set in the public right-of-way, combined with carefully selected landscaping, can create memorable entries into the Westside district.



Decorative light standard.



Drinking fountain.



Street clock.



Information kiosk.



Decorative bicycle racks.



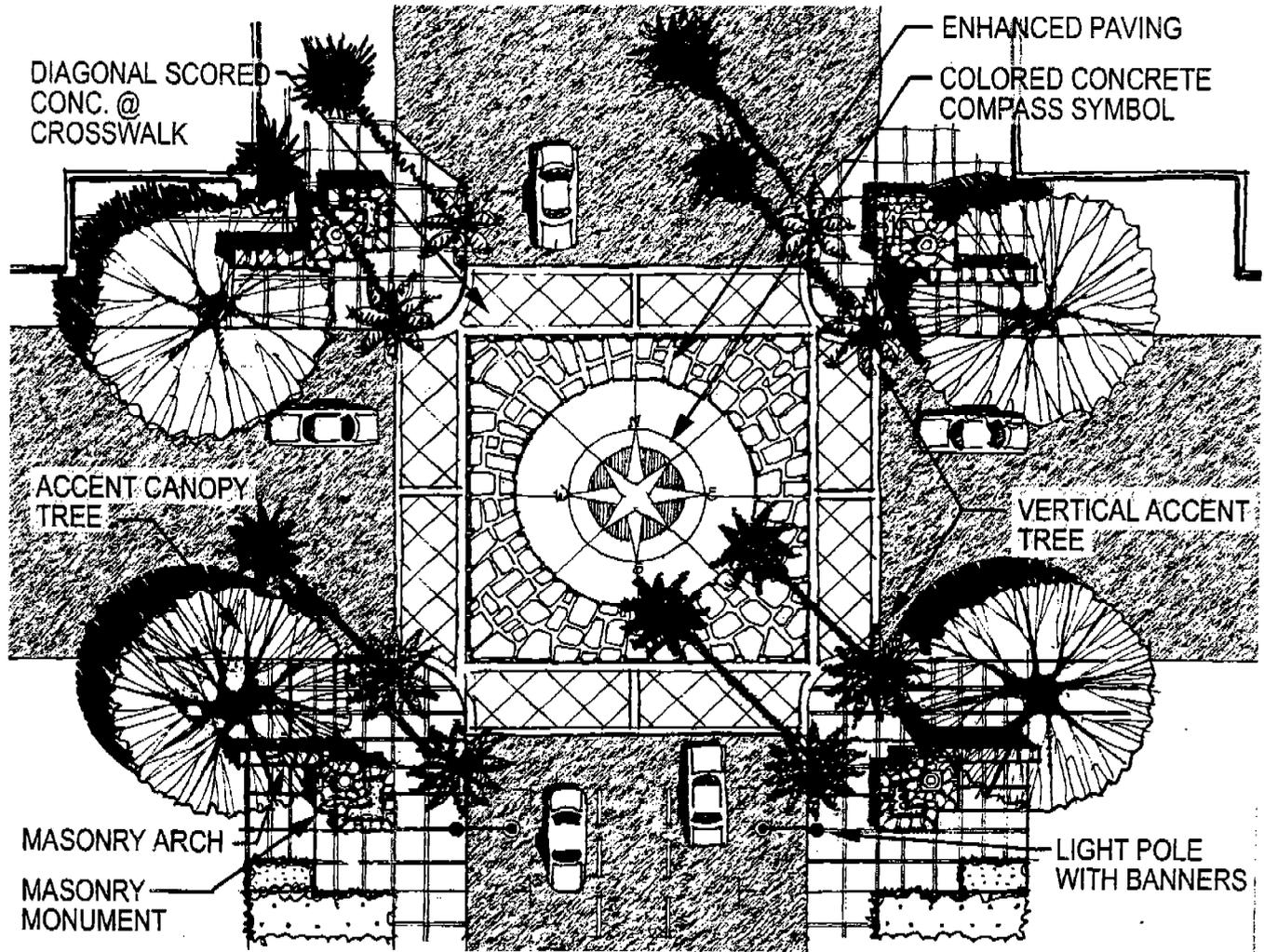
*Large planter.
Typical street entourage.*



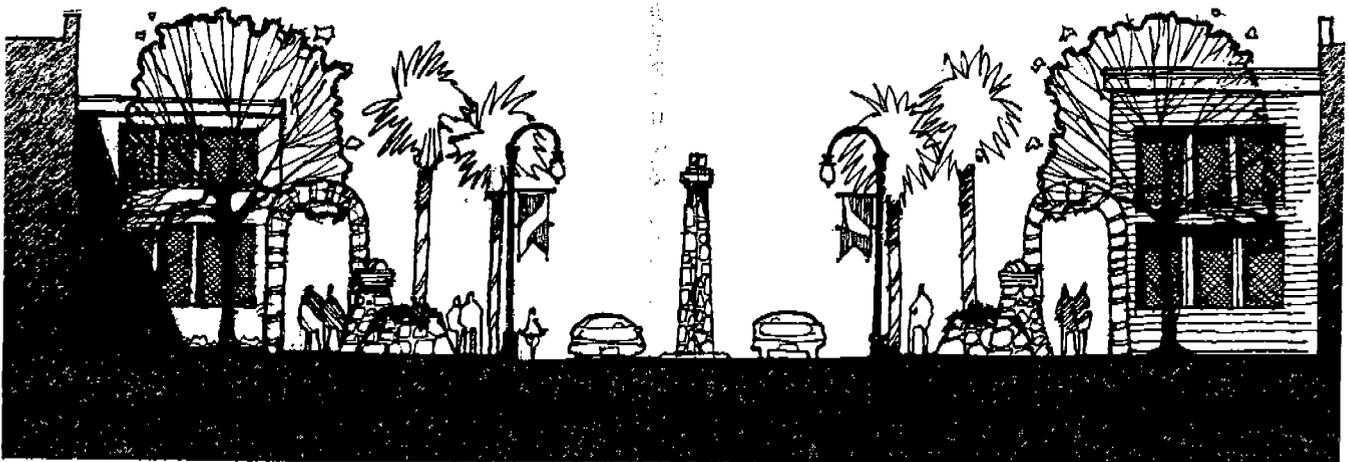
Street lights and enhanced paving.



Enhanced paving.



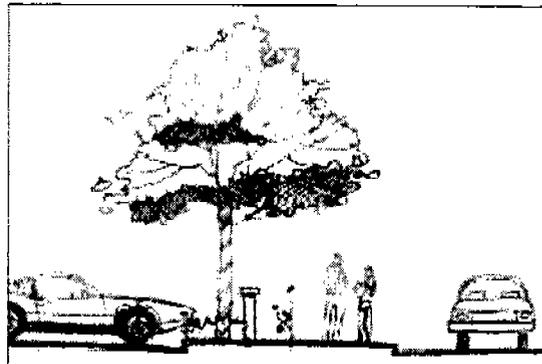
Major intersections which mark the point of entry into the Westside should be enhanced to create a strong sense of transition to this district. Rather than rely on strictly signage to mark the gateways, special street and sidewalk paving, formal landscaping, street furniture and lighting standards and special monumentation will give the visual cues announcing arrival at the district.



This street section shows the monumentation at the gateway, including the possibility of a vertical marker or obelisk set in the street. A change in the light standard type and the addition of banners also indicate a change of district.

Suggested Landscape Elements

The principal plantings should be tree rows, which define the public space. All streetscapes should be planted with 24-inch box sized trees. Consistency in tree species and spacing should be used to establish a strong street identity. Shrubs, vines, and groundcovers should be mostly prostrate flowering varieties and should be planted at gateways, parking areas and public spaces.



Masonry screen walls should be provided between vehicle parking and the Streetscape area..

VENTURA AVENUE

Street Trees

Trees of transparent, vertical character should allow signs on retail establishments to be seen from the traveled way. Planted no closer than 30 feet and no further than 40 feet on center, the trees should be planted in 42" square wells. Tree wells should be covered with tree grates in the focus areas and may also have tree guards.

Accent Trees at Ventura Avenue Intersections

Flowering trees / four per intersection
Planted in wells with tree grates and tree guards. Accent trees should be lighted from spotlights on buildings or "uplights" encased in tree grates with protective shields.

Accent Trees at Intersections

Jacaranda Tree / four per intersection

ALLEYS

Wherever possible, alleys should be detailed as off-street parking with landscaping, screen walls, and enhanced paving. Walls should be masonry and no less than 42 inches high.

Vines should be considered for adjacent building walls and masonry screenwalls.

Alley trees

Hong Kong Orchid Tree
Jacaranda Tree

Sweetshade Tree
Crepe Myrtle Tree



Proposed residential street trees.

Wherever possible, alleys should be detailed as off-street parking with landscaping, screen walls, and enhanced paving.



Proposed residential street trees.



Alternative public right-of-way plantings in narrow street situations.

GATEWAYS

Gateway Trees

Jacaranda	Canary Islands Date Palm
Queen Palm	Gold Medallion Tree
Silk Floss Tree	

Gateway Shrubs & Groundcovers

Bougainvillea	Lantana montevidensis
Pittosporum t. "Wheeler's Dwarf"	

Gateway Vines

Bougainvillea	Royal Trumpet Vine
---------------	--------------------

PARKING AREAS

Parking areas should receive landscape treatments which achieve screening of parked vehicles, and which provide visual continuity to adjacent streetscape and landscape themes. Wherever possible, 42" high masonry screen walls should be provided between vehicle parking and the Streetscape area.

Parking Lot Trees

Hong Kong Orchid Tree	Jacaranda
Crape Myrtle Tree	Tipu Tree

Parking Lot Shrubs & Groundcovers

Bougainvillea	Prostrate Rockrose
Prostrate Cotoneaster	Purpleleaf Euonymous
Trailing Lantana	Prostrate Tobira

Parking Lot Vines

Royal Trumpet Vine	Blood Red Trumpet Vine
Madagascar Jasmine	Bougainvillea

Parking Lot Hedge Plantings (When used in lieu of a wall)

These hedges should be pruned and sheared into a solid hedgerow of the specified height, without breaks or openings between individual shrubs. Acceptable species are:

- | | |
|-----------------|----------------------|
| Pink Escallonia | Wax Leaf Privet |
| India Hawthorn | Japanese Boxwood |
| Red Photinia | Shiny Leafed Xylosma |

Parking Lot Trash Enclosures and Streetwalls at Loading Areas

These walls should be planted with self-adhering vines no less than 10 feet on center and a minimum of 5 gallons in size. Acceptable species are:

- | | |
|--------------|------------|
| Creeping Fig | Boston Ivy |
|--------------|------------|

PLAZAS AND OFF-STREET PUBLIC AREAS

Plazas and other focal point nodes should be planted with medium to small trees with an open transparent character so that signs and/or other graphics might not be screened from view of passing motorists, and pedestrians.

Plaza Trees

- | | |
|-------------------------|-------------------|
| Queen Palms | Mexican Fan Palms |
| Thornless Honey Locusts | Aristocrat Pears |
| London Plane Trees | Crape Myrtle |

Plaza Shrubs

Shrubs should be limited to low-growing, prostrate varieties with color accents. Large potted planters could contain dwarf citrus for color and as reflections of the agricultural past.

- | | |
|--------------------------|-----------------|
| Multi-colored lantana | Star Jasmine |
| Dwarf Tobira Pittosporum | Carmel Creeper |
| Dwarf Kumquat Citrus | Dwarf Tangerine |
| Dwarf Orange | Dwarf Lime |



Bailey's Acacia.



King Palms.



"Transparent" trees, such as a honey locust, provide some shade while allowing elements like signage to be seen.



Lighting in the core area can be used to create dramatic effects in public areas.

General Landscape Development Standards

The minimum landscape requirements that should be met in the design of all improvements on public and private land within the Westside District should include the following:

Standards

In addition to City of Ventura Landscape Standards and guidelines, and the landscape provisions of this document, the following should apply:

- All areas not devoted to paving or building should be landscaped and permanently maintained.
- To complement building elevations, a landscape area should be provided adjacent to facades and side elevations wherever possible. Planting area dimensions should be consistent with plant material requirements and the purpose of the planting, such as aesthetics, screening, environmental mitigations, air quality, wind, etc.
- All landscaped areas are to be delineated with minimum 6-inch concrete curbs or equivalent as approved by the Parks Manager of the City of Ventura.
- Permanent automatic irrigation facilities should be provided in all landscaped areas.
- Prior to the issuance of building permits, a landscape and irrigation plan in conformance with these Guidelines should be submitted to the City of Ventura Parks Manager.
- To minimize exterior water use, drought-tolerant plants should be utilized in all landscaped areas, and low precipitation rate irrigation systems should be utilized wherever possible.
- All street trees should be planted and staked per City of Ventura Landscape Standards and Details. All trees planted in turf areas should receive turf boots to prevent damage from mowers and edgers, etc. Root barriers should be required where trees are planted within five (5) feet of any walls, curbs, walks, buildings or other hardscape.
- All plant materials should be planted in the following sizes and should be in accordance with all City standards and minimum requirements.

Trees

All trees should be a minimum size of 24" box. All trees at each side of designated gateways should be a minimum size of 48" box.

Shrubs

Shrubs located at entries and major intersections should have a minimum size of 5 gallons with minor exceptions as approved by the City of Ventura Parks Manager.

Drought-tolerant plant material

Should be defined as plants listed as Moderate, Low, and Very Low in the WUCOLS PROJECT listing of Water Use Classification of Landscape Species as published by the University of California Cooperative Extension.

University of California publications may be obtained at:

ANR Publications

University of California

6701 San Pablo Ave., Oakland, CA 94608

Telephone (415) 642-2431



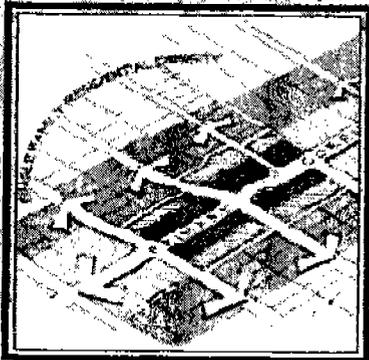
Planters are an effective way of providing landscaping on paved areas.



Tree grates provide hardscape continuity while allowing water and air to the roots.



The Private Realm



DESIGN FOR THE PRIVATE REALM

As previously stated, one of the key issues to be addressed in this document is the creation of private property design guidelines for the Westside, to reinforce the positive character of what is already built and to guide new construction compatible with this existing character. Understanding the built context of the Westside and responding with new construction which "fits in" is critical to attaining the Vision that the community has for itself.

The long-term success of any area improvement is, to a large extent, dependent on the degree to which private property owners commit to constructing or upgrading their sites in step with the community vision. Upgrading the physical appearance of a property does not automatically guarantee immediate economic return. However improvements within the district will have a cumulative positive impact which will likely benefit all stakeholders over time. Furthermore, property improvement tends to be a "contagious" activity. When one business or property owner makes positive improvements in a building's appearance, adjacent owners often respond in kind.

PEDESTRIAN CORE AREAS

Intent of the Designation

The designation of 'Pedestrian Core Area' (PCA) at strategic locations along the Ventura Avenue corridor is neither an exclusive designation nor an attempt to radically alter the character of the places within the boundaries. It is not exclusive in that similar development is encouraged along most of the length of Ventura Avenue. However, within these Pedestrian Cores, particular attention is given to improvements, both public and private, that enhance street-level pedestrian activity, such as ground level retail, restaurants and other neighborhood serving uses.

In addition, mixed-uses are strongly encouraged within these core areas - uses which combine daytime and evening commercial activity with the twenty-four hour presence of residential uses. This dynamic mix of uses will help to assure that businesses are supported by local users who have easy access to their services, that limited land receives appropriate



“Property improvement tends to be a “contagious” activity. When one business or property owner makes positive improvements in a building’s appearance, adjacent owners often respond in kind.”

intensity of use, and that the twenty-four hour-a-day presence of residential stakeholders will increase the security of the area for all concerned.

Transit-Oriented Development

“One of the key benefits resulting from the establishment of the “core areas” is that automobile trips are likely to be reduced, as residents begin to enjoy the convenience of “access by proximity.””

Unlike many ‘marginal’ commercial districts, the Ventura Avenue corridor does not suffer from a lack of traffic, both vehicular and pedestrian. In fact, many within the community feel that this corridor is overused. Because of the somewhat haphazard interspersion of commercial, residential and industrial uses, many stretches of Ventura Avenue are heavily used by automobile, bus and truck traffic. One of the key benefits resulting from the establishment of the “core areas” is that automobile trips are likely to be reduced, as residents begin to enjoy the convenience of “access by proximity.” This is one of the key concepts behind the strategy of Transit Oriented Development - or “TOD.” These core areas will provide a variety of services and jobs, as well as comfortable access to bus transit.



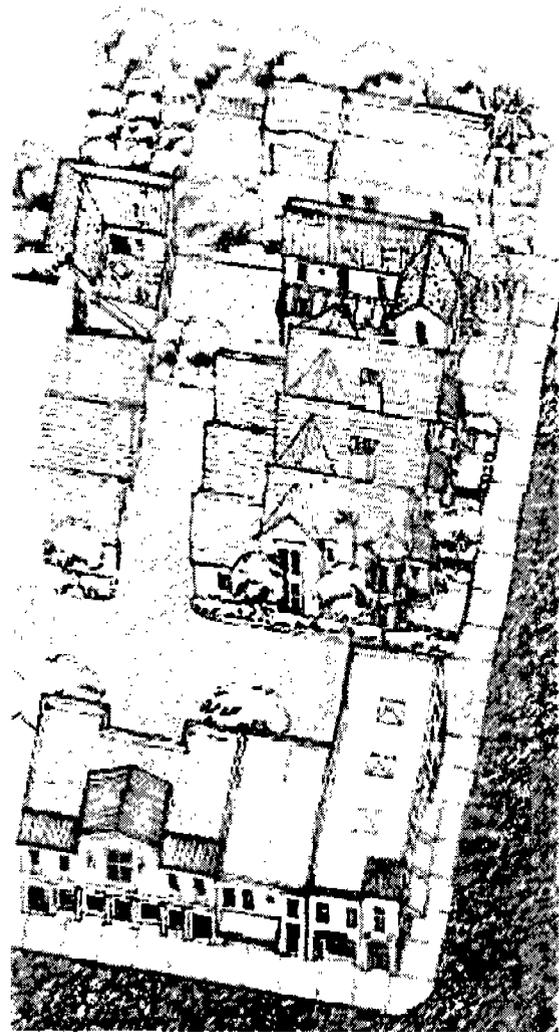
In a diverse context, new design should strive to complement, not copy, existing buildings.

Character of the Pedestrian Core Areas

The Pedestrian Core area is intended to provide for a healthy mix of commercial and residential uses. In some locations this will result in an intensification of uses potentially bringing more foot traffic into the area. It is intended that improvements in the district will result in an influx of retail businesses and services and that these establishments will enhance or construct their storefronts to be welcoming and even encourage people to get out of their cars and walk. Although parking should not be eliminated from the Pedestrian Core area, it should in many ways be subjugated to the interests of the pedestrian. As such, the 'strip mall,' using parking as a buffer between the retail and the street, is not a welcome addition in the future of the PCA. In addition, parking strategies for public lots and/or shared access agreements between adjacent parcels will likely become important as the PCAs become established.

Most areas receiving designation as 'Pedestrian Core Area' were not selected to be radically transformed into something else (an exception would be the Ventura/Stanley PCA, which is being created to serve the newer neighborhoods in the north). In fact, these areas are currently Ventura Avenue's most vital districts. These design standards are intended to bolster the existing building stock with infill and renovation projects that reinforce and ultimately contribute positively to the overall ambience put forward as the Community vision.

It should be stressed that there is no single stylistic approach recommended for new construction in the Westside. One of the unique architectural qualities of the commercial areas along Ventura Avenue is the variety of styles. This variety is a testament to the historical and cultural growth and change that has occurred and should be enriched in the future. The elements of design discussed in this document strive to establish a rich architectural and spatial experience for the pedestrian/shopper/resident while allowing for a wealth of diversity and visual interest for the larger community.



"It should be stressed that there is no single stylistic approach recommended for new construction in the Westside. One of the unique architectural qualities of the commercial areas along Ventura Avenue is the variety of styles."



It is not just its style, but also the way this building addresses the street and corner, which makes it a successful architectural gesture.

Uses Within the Core Areas

Commercial

The PCA, in order to emphasize pedestrian-friendly street frontage, should have exclusively commercial retail uses at the ground floor level at the street. Multistory buildings can have more commercial retail on the upper levels or have a mixed-use occupancy that includes residential.

Mixed Use

Commercial/ Residential

Residential units above commercial were a common configuration in older American cities where land was a premium and the ownership of automobiles was not commonplace. Advantages of residences within commercial areas include proximity to shopping or place of employment, an after-hours presence and less reliance on automobiles.

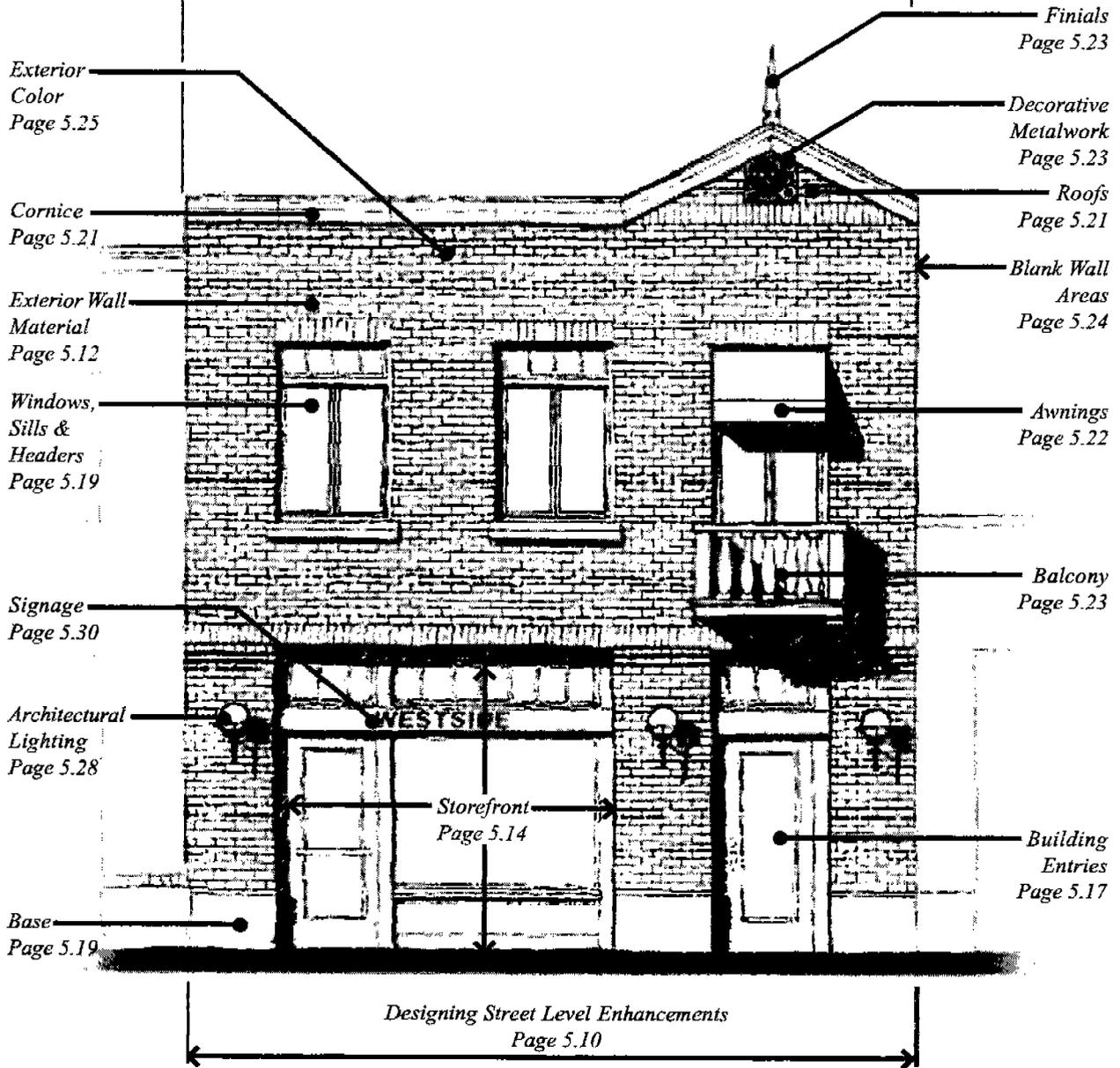
Commercial/ Office

Neighborhood-serving businesses, such as travel and insurance agents, doctors, attorneys, etc. and many small businesses find that office space in these urban areas can be conveniently reached as well as having associated businesses (copy shops, banks, restaurants) nearby.

Live/ Work, Artist Lofts

Live/Work is a relatively new term for an old concept in which a business proprietor resides above his or her shop. This is often a desirable situation for security and also longer hours of operation. There is a thriving art community in the Westside, so the establishing of Artists Lofts, in which an artist resides, conducts their particular trade and may also sell their wares, is a unique possibility.

Building Mass and Organization
Page 5.8



KEY ELEMENTS OF BUILDING DESIGN



What opportunities exist to encourage or strengthen pedestrian activity?

“It is a fundamental necessity for the preparer of the design to study the context of the proposed project.”

BUILDING FAÇADE DESIGN AND COMPOSITION

The intention of this document is to discuss approaches to architectural design that transcends 'style.' Virtually all popular styles employ elements consistent with pedestrian-oriented design, whether traditionally-rooted or new and avant-garde.

It is a fundamental necessity for the preparer of the design to study the context of the proposed project. While not every condition can be anticipated here, the following issues are important to consider:

- What opportunities exist to encourage or strengthen pedestrian activity?
- Can ground level uses contribute to or establish continuous retail activity in the district?
- Do neighboring existing buildings, which are deemed to be consistent with these guidelines, have characteristic qualities, such as style or ornamentation, which may influence the design?
- Can the design enhance or strengthen an adjacent building of fine design or historical importance?
- How can the sidewalk environment be improved? Can this be achieved by simply using creative painting or elements such as awnings or potted plants?
- Is there opportunity for special corner treatment?
- If mixed-use is proposed, how can the different aspects be expressed in the façade design?
- Are there any opportunities presented by the adjacency of a plaza, park, natural area or other open public space?
- Can security issues be addressed by design? Can the *perception* of safety also be enhanced?
- If vehicle access is required to the site, how can it best be configured to maximize the desired pedestrian-friendly environment and not be visually detracting?

Building Mass and Organization

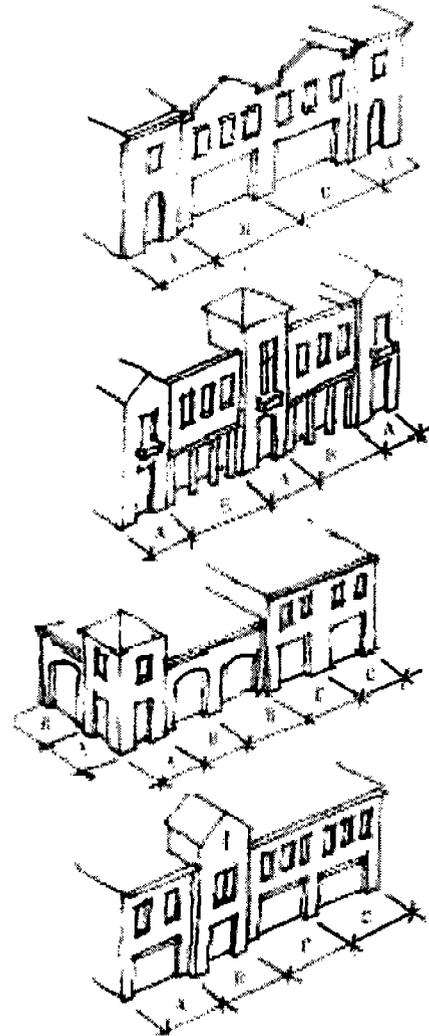
The PCA typically incorporates a variety of buildings of differing shapes, sizes and styles, and includes buildings with pitched roofs, flat roofs, decorative parapets, mansards, etc. But there are also significant design similarities which should be sought and supported, particularly in the manner by which the building addresses the street, the floor-to-floor height, and the rhythm established in the pattern of openings for storefronts, doors and windows.

Current economics often deem larger building development as more feasible, rather than developing the existing small parcels on a piecemeal basis. With this understanding, construction of larger buildings within the district need not be discouraged, providing that certain design considerations are incorporated to maintain contextual compatibility. To assure that new and renovated buildings are compatible with the existing character and scale of the district, such projects should be organized into visible "building increments" of no less than twenty-five (25) feet and no more than fifty (50) feet in width. Buildings which occupy more than fifty feet of frontage should be designed to appear as several small buildings or several smaller but related parts of a larger structure. This can be accomplished in several ways, such as: incremental changes in roof eave, changes in wall plane, grouping of windows into varying or repetitious patterns and coordinated placement of design elements, including walls, windows, fixtures and storefronts.

Building heights

Heights should generally not exceed two stories, with average ground level floor to ceiling heights ranging between eleven (11) and fourteen (14) feet. The upper portion of two story buildings should be designed to reflect a refinement of the incremental rhythms and pattern of openings found at the street level of the building facade.

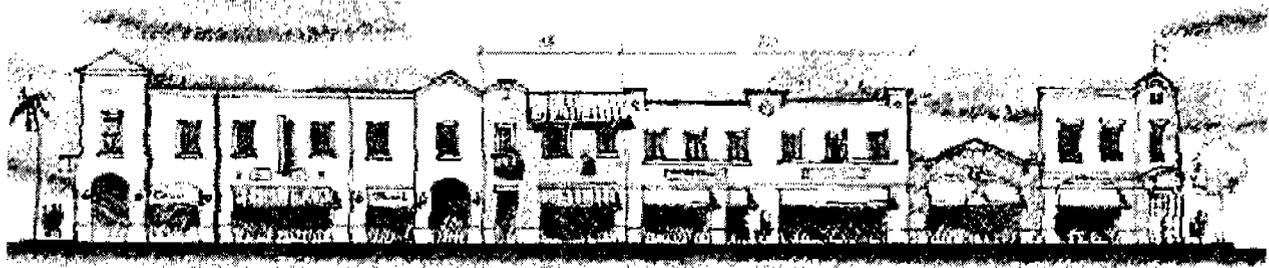
The primary components at the upper story, such as windows, wall panels, projecting bays, etc., should typically be grouped in a manner consistent with the building increment established at street level.



Proportion and rhythm are the basic essential design tool for a successful facade design.

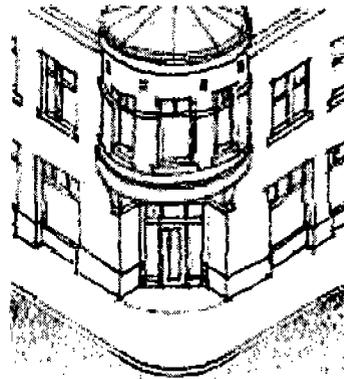
Good proportion is the harmonious relationship between design elements to the whole as well as the elements to each other.

Rhythm is the repetitious pattern of design elements, particularly of 'bays', as illustrated here.

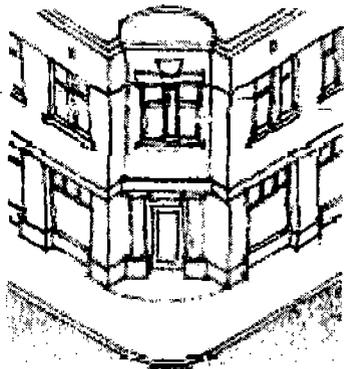


Corner Conditions

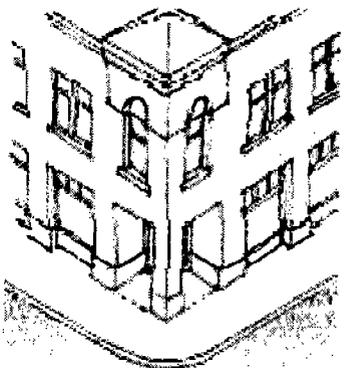
A corner location provides a unique opportunity to announce a building's presence and establish a memorable point of reference. Building corners may be configured in several different ways, a few of which are illustrated here.



Projecting turret



Re-entrant corner



Corner tower

Designing Street Level Enhancements

Design of façades should have two approaches: the overall façade composition should follow design parameters of scale, proportion, rhythm, etc. and the individual storefronts at the street level should have elements of individual expression. These two approaches converge to attempt to provide *visual continuity* within the block (and the District as a whole) while allowing *visual variety*, therefore maintaining interest, at the pedestrian level.

The following guidelines are recommended:

- Individual shop or tenant spaces should be expressed on the building exterior through the rhythm of openings, wall and column, as well as careful use of color and material changes, where appropriate;
- Where possible, at least one building entrance should be provided every twenty-five (25) to fifty (50) feet to encourage a high level of pedestrian activity at the street;
- Larger buildings should be designed to appear as a collection of adjacent structures when street frontage exceeds fifty feet, and this should be clearly expressed at the street.

Consult further into this chapter for design of specific elements.



A simple, yet texturally rich, facade is often an effective one.



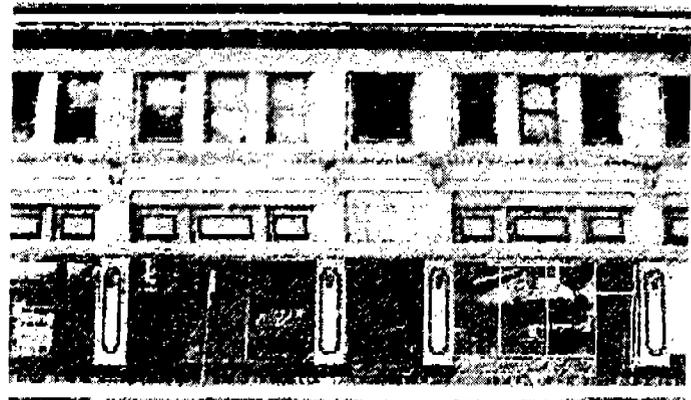
Upper level design should correspond to lower levels. In this example, the grouping and size of windows are similar. The treatment around the windows is very different; more ornate at the second level and simpler at the top level. Note how the windows relate to the rhythm of the storefront bays.

Designing Upper Stories and Roofs

While certainly the emphasis of the PCA is on ground level activity and how design should enhance it, upper building levels are important to enhance the Core area as well. High building walls along the street enhance the sense of enclosure and provide shade. Upper story uses, such as residences, bring people to a district to use the retail shops and are after hours 'eyes on the street.'

The following guidelines are recommended:

- Upper story design should correspond to lower level and street level facade design through alignment of openings, similar window patterns, etc.;
- Similar materials and ornamentation give visual unity to the whole facade;
- Horizontal elements from adjacent buildings, such as mouldings or cornices can be used as design cues;
- Visible roof elements should be designed consistent with the general building character. Materials and colors should complement the style of the architecture.



Exterior Wall Materials

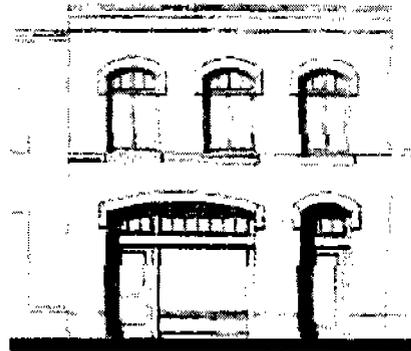
Material for exterior walls should incorporate two aspects - color and texture. If the building's exterior design is complicated with many "ins and outs," columns, and design features, the wall texture should be simple and subdued. However, if the building design is simple (perhaps more monolithic), a finely textured material, such as patterned masonry, can greatly enrich the building's overall character.

The following materials are considered appropriate for buildings within the Core area. However, the number of different wall materials used on any one building should be kept to a minimum, ideally three or less:

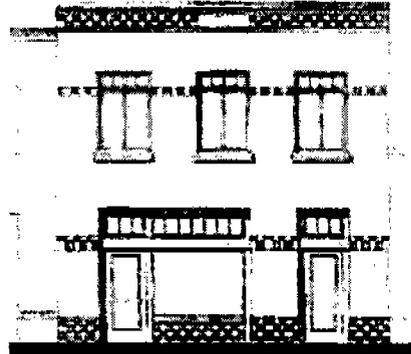
Brick Masonry

Brick is a very versatile material available in a wide range of colors and sizes. This material offers a human scale and comfortable, familiar texture. Brick surfaces may be patterned by combining different colors and coursework. Traditional brickwork incorporates a wide array of detail applications expressive of structural and functional building components, such as beams, headers, arches, sills, bases, trim, etc.

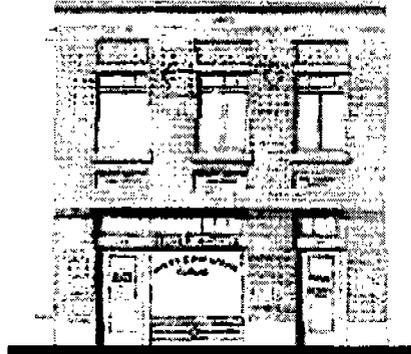
- Application - in new construction in California, brick will almost always be used as a veneer or finish material over a frame of wood or steel, or as a facing for concrete masonry. Care should be taken to properly detail the veneer application in an authentic manner consistent with traditional masonry design. Edges and openings, such as at windows, should receive special care to assure that the masonry appears solid and substantial.
- If "thin brick" or brick tile veneer is used, care should be taken to incorporate corner pieces and other details which serve to mask the veneer application.
- The bonding pattern (the orientation of the brick) plays an essential role in the successful use of brick or other masonry. Bonding patterns of the existing historic context are typically very simple.



Stucco with simple stucco relief details.



Stucco with tile details and accents.



Brick with brick relief, soldier coursing and masonry base..

Stone Masonry (and Veneer)

Stone masonry, especially cut stone, is similar to brick in application and provides a very heavy and somewhat formal appearance. Stone also works well in combination with brick, providing a base with visual mass to "support" walls of brick masonry. Stone may be used as an inset detail on brick facades for sills, headers or other accent features, such as keystones or wall caps.

Stucco

Stucco is a very versatile material. With proper detailing, a stucco building can achieve elegance. In fact, the traditional "stucco" building was a masonry structure with an application of stucco applied as a finish material. For this reason, the stucco building should also convey a sense of mass and sturdiness similar to a brick or other masonry building. The same attention to detail applies here as with buildings of brick or stone.



Elaborate detailing can be achieved even by using stucco.

Terra Cotta and other Glazed Tile

Terra Cotta tile was a common finish material for urban buildings in the early part of this century and has staged a recent "comeback" owing to its lasting beauty, timeless appearance and quality. Other glazed ceramic materials, such as glazed tile, block or brick, may also be considered for use as an exterior wall finish. These types of materials have a hard, stain-resistant surface which makes an excellent base material. Only glazed materials graded for commercial use should be considered for use as an exterior finish material.

Wood Siding

Wood siding can be used as part of the architectural vocabulary in combination with other traditional design elements, such as columns, posts and exterior wood casework. However, wood is more vulnerable to damage, requires more maintenance and is less durable over time than other materials, and this should be considered in the design.

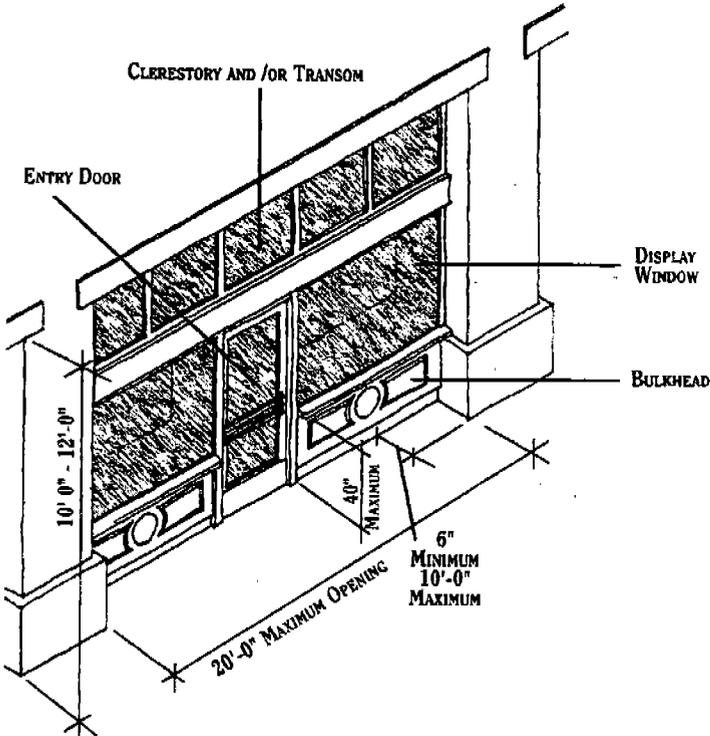
Storefront Design

Interesting and enticing storefronts are perhaps the most crucial ingredient in promoting a vital and active street life in a commercial district. Storefronts should be generous, providing ample display window and entry points into shops, and a level of design detail that establishes some individuality for each shop or building, while assuring a relationship between structures.

As previously stated, a building entry provides the opportunity to create a detailed point of arrival for pedestrian users. This portion of the building will receive more personal contact by users than any other on the building exterior. Thus, what is seen and touched should be of attractive and durable quality.

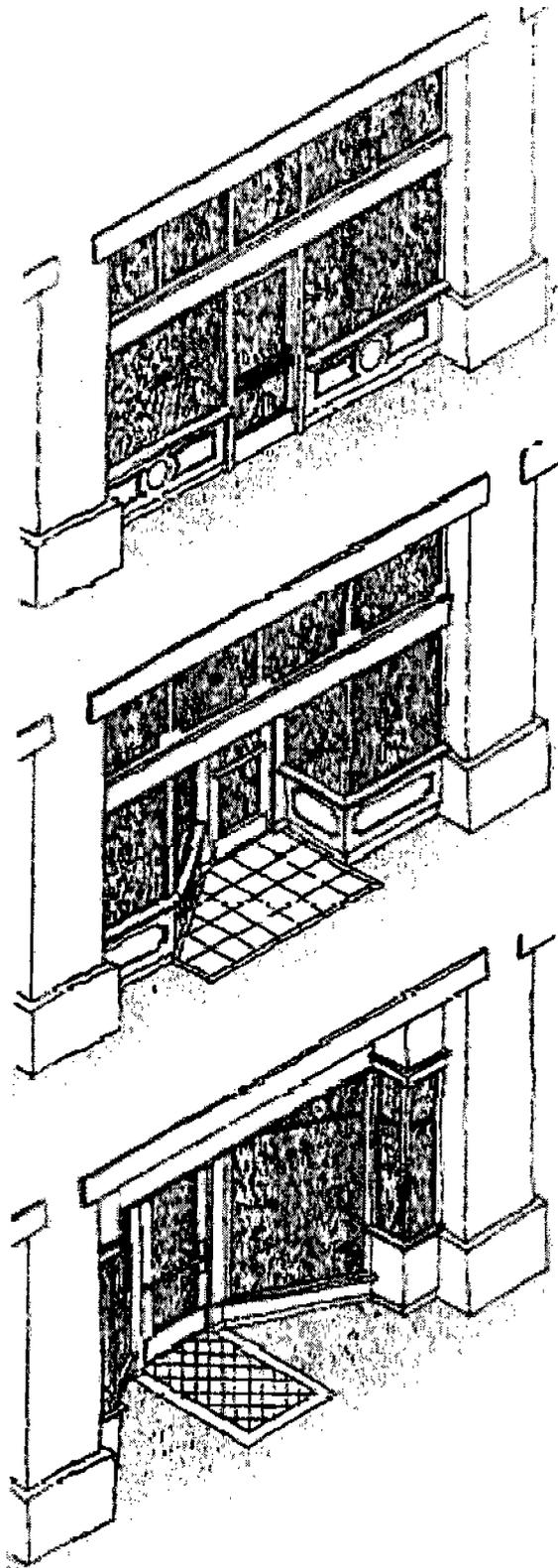
Storefront Width and Height

Storefront openings should not exceed twenty (20) feet in width. Many buildings in the Pedestrian Core district have much smaller storefront openings, ranging from approximately eight (8) to fourteen (14) feet.



“Interesting and enticing storefronts are perhaps the most crucial ingredient in promoting a vital and active street life in a commercial district.”

Storefront Elements



Numerous variations for storefronts are possible.

Storefront openings may extend up to fourteen (14) feet in height, but in general should range from ten (10) to twelve (12) feet in height, as measured from average elevation at the sidewalk.

Recessed Entries in Storefront

The common feature in traditionally designed storefronts is the recessed display and entry area. This recess serves several valuable purposes: it provides weather protection at the entry; it expands the linear feet of window display that a shop may present to passersby; and, along with awnings, it helps protect displayed merchandise from costly damage by direct and prolonged exposure to sunlight.

Maximum width of such recesses should not exceed sixty (60) percent of the storefront opening and maximum depth of such recesses should not exceed ten (10) feet.

Where the entry is recessed, a decorative paving material, such as tile, marble or slate, may be used adjoining at the sidewalk edge.

To punctuate an entry, decorative lighting may be incorporated, either wall-mounted or suspended from an ornamental ceiling.

Storefront Details and Materials

In general, storefront systems should be of high quality and durable construction with traditional detailing, including moldings, beaded glass stops, paneled soffits, sills, etc. Materials may include wood or metal, always exterior grade, and of a finished quality, typically painted or factory finished. Raw colored aluminum is not recommended.

Recess

Storefront window systems should be set into the building face. This will allow the building face material to return at the corner of the storefront opening, establishing a strong sense of mass and substance to the building.

Design Elements for Storefront Systems

Traditional storefront systems are made up of several parts which, when integrated, provide the opportunity for variation in detail and character. Storefront systems should address the inclusion of most or all of the following elements:

Bulkhead

At the base of the storefront which extends down to grade, the bulkhead provides a transition between the sidewalk or paving and the glass display area. Bulkhead may be of wood panel, tile, or masonry. Base materials should be compatible and consistent with the building design. Where wood is used, a curb detail should be provided to lift the wood slightly above the adjacent paved surface. The Bulkhead and associated trim should not extend higher than forty (40) inches above adjacent paving, to assure ample window area visible to pedestrians.

Display Window and Glazing

These should be generous and highly transparent. Windows should occupy sixty (60) to seventy (70) percent of the storefront system area. Multi-pane windows may be used, but are not encouraged for the majority of display window serving retail shop space.

Dark tinted windows or windows of reflective "mirrored" glass are strongly discouraged, as they hinder pedestrian "window shopping." Where sun control is a design factor, windows should be recessed and/or provided with awning or other shading device.

Transom Windows

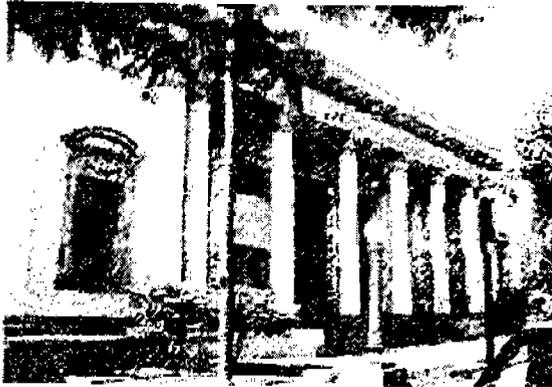
These are upper glazed openings, above display windows, which are primarily intended to provide light to the interior of the shop space. These windows typically rest atop an intermediate trim or header which is exposed or trimmed-out with sill and glazing stops. Sometimes this beam is oversized to allow for shop signage to be placed integrally within the storefront system.

Transoms are recommended for street-level shop space in all commercial buildings within the Pedestrian Core, and should be an integral part of the building's storefront system.

Transoms need not be of transparent glass. There are a number of translucent glass types which will admit light while reducing heat gain and glare.



Attention to detail, not necessarily elaborate, makes a welcoming retail storefront.



Building entrances can be grand or subtle depending on the a variety of criteria, including building use.



Corner entries provide exciting opportunities for an entrance. Here the elaborate second floor corner bay window provides cover and accentuates the main entry.

Building Entries

In most cases entries are encouraged to be placed a maximum of twenty-five feet apart within the Pedestrian Core Area. This will help maintain the intimate pedestrian scale and ensure lively pedestrian activity.

Main Entrances

Main entries to the street level shops should be emphasized one or more of the following ways:

- Flanked by columns, decorative fixtures or other details;
- Recessed within a larger arched or cased decorative opening;
- Covered by means of a portico (formal porch) projecting from or set into the building face (refer to zoning guidelines for allowable projections);
- Punctuated by means of a change in roof line, a tower, or a break in the surface of the subject wall.

Corner Entrances

Buildings situated at the corner of a public street may provide a prominent entrance to street level shops or lobby space, in a manner consistent with Main Entries, as described above.

Side and Rear Entrances

Where such entries are intended to serve the general public, they should receive design treatment similar to that described above. Where such entries are intended only as service or emergency access points, they should be designed consistent with guidelines for Service Entries described below, except that any such entry which faces onto a public street or right-of-way should be enhanced, though to a lesser extent than a main entry.

Service Entrances

Service entries are those entries that are not intended for use by the general public.

The service entry should be designed to blend into the surrounding building facade, provided with simple detailing, trim and finish consistent with the character of the building, such that it should not draw particular attention to itself, but should remain attractive and easy to identify.

Where trash and or storage areas are provided in conjunction with service entries, such areas should be completely enclosed and screened from public view by the use of walls, doors and gates designed in a manner and with materials consistent with the architecture of the buildings.



An elaborate canopy enhances an otherwise simple building corner.

Architectural Elements

These elements make up an important part of a building facade's composition and are the connectors between the life and activity on the street and that within the building. These "sensed experiences" are important in establishing an urban environment that is comfortable and friendly. Because of this, attention to design and detail is important.

Building base

Where the building meets the sidewalk, and extending two (2) or more feet up the building face, is the opportunity to establish an architectural base. This base may be as simple as a change in surface texture, a projection or break in the wall plane, or a change in material or color.

It is desirable to provide a base material that is highly resistant to damage, defacing and general wear and tear. Cast-in-place concrete, precast decorative concrete, stone masonry, brick and commercial grade ceramic tile are examples of excellent base materials. Hollow core fiberglass mesh reinforced precast concrete should not be used as a base material, as it is susceptible to breakage and puncture.

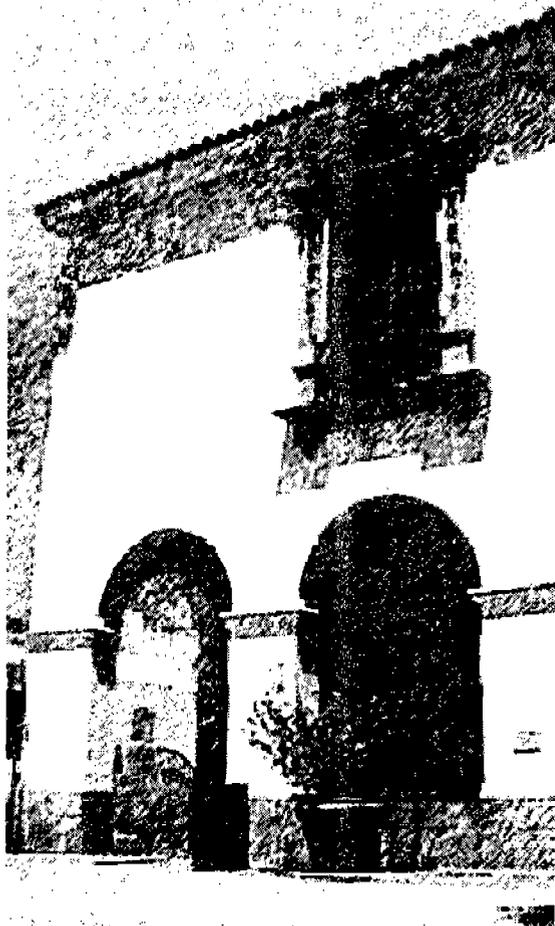
Cornices and moulding

If used, these elements should unify and connect the composition of the building facade. They should be simple in character and used sparingly, consistent with the specific architecture design of the building and other buildings of the Pedestrian Core area.

Windows, Doors and Openings

Doors should be decorative and substantial, with high quality and very durable hardware. Commercial grade materials should be used to accommodate long-term wear.

- Wooden doors should be provided with a 'kick plate' of brass or other durable material to resist wear at the door base.
- Aluminum or metal doors framing glass, or which



In this example, a simple stucco building is elegantly accentuated with a stone base, a line of similar stone articulating the arch spring line and a projecting cornice that casts a deep shadow on the building. The simplicity sets off the richly ornate second floor window which features projecting mouldings at its sill and head.

incorporate decorative panels, are encouraged. Plain slab aluminum or metal doors are discouraged.

Where doors are recessed, decorative ceilings, with beamwork, moulding or coffering should be incorporated;

Windows and other openings should be detailed in a manner compatible with the overall building style, and to establish them as important parts of the total facade composition.

Sills, headers and mouldings can be used to frame and enhance the significance of an opening. In some cases an opening may warrant greater attention, such as at or above an entry. The moulding and header may be enlarged or receive special detail in order to signify its importance in the overall facade composition.

Window Ratio

The amount of window, other than storefront systems, at street level should occupy between 60% and 70% of the wall area. This will help assure that there is architectural expression at the street level including ample display and visual interest for pedestrian shoppers.

At second floor levels and above, this ratio should fall to between 30% and 50%, depending upon building style and other architectural criteria.

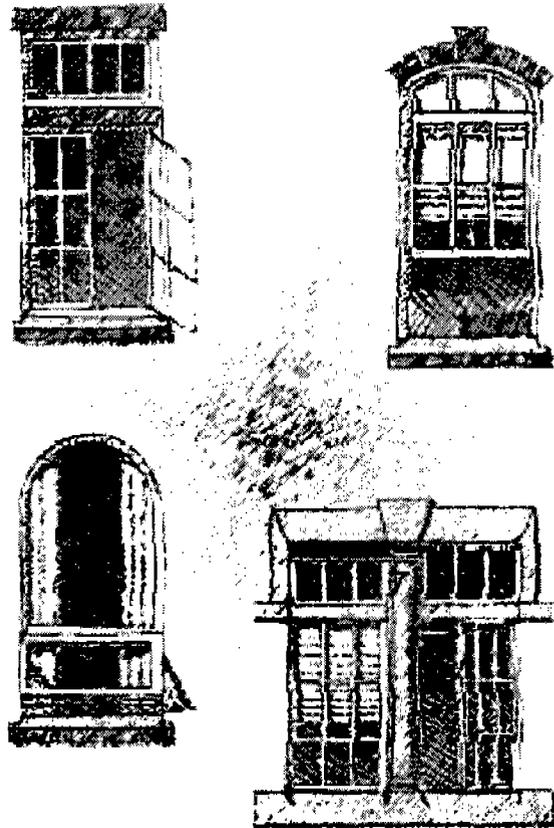
Materials

Window sash and frame may be of wood or metal, painted or otherwise finished to provide a durable and long lasting surface. Where divided windows are desired, true-divided lites are strongly recommended. "Snap-in" type mullions or grilles that simulate this appearance are not recommended.

Glazing

Clear glazing is preferable, especially where street level display is the objective. Reflective or mirrored glazing is strongly discouraged.

Where tinted glazing is used, the tint should be kept as light as possible. Dark windows create the impression of emptiness. "Low E" coating, which appears clear, is recommended where heat gain is a concern.



Windows are the connectors between the life and activity on the street and that within the building. They can be elaborate or simple, depending on the composition of the whole facade.



The underside of this projecting cornice is detailed in accordance with the style of the building. It provides visual interest to the pedestrian.

Cornices and Parapets

Where appropriate, eaves, cornices and parapet walls are encouraged to delineate the building profile in a manner consistent with the architectural style of the building.

Buildings should incorporate the following design elements:

- Parapet walls, incorporated to establish an interesting profile at the street elevation(s);
- Cornice treatment, as a design finish to visually "cap" and decorate parapet walls.
- Where there is roof top equipment, parapet walls should be high enough to screen such equipment.

Roofs and Eaves

Visible roof elements should be designed consistent with the general building character. Materials and colors should complement the style of the architecture.

Pitched Roofs - should be designed to crown or punctuate the building. Roof elements should terminate with detailed eaves, utilizing eave molding, shaped rafter tails or other means of termination appropriate to the architecture of the building. Where pitched roofs are used, the building should incorporate the following:

- Eaves of pitched roofs should join the building walls with a decorative eave cornice complementary to the design of the building face.
- Where overhang is provided, eaves should present either exposed rafter tails with decoratively shaped ends or a decorated soffit with soffit vents incorporated into the design.
- Plain and unadorned stucco eave soffit should be avoided.

Mansard type roofs should be avoided, unless specifically related to the architectural style of the building (as in the "Victorian" style).

Dormer type roof elements may be incorporated into sloped

roof designs, especially where such roof area occupies a substantial percentage of the building's linear frontage.

Acceptable materials include:

- Clay or concrete tile: commercial grade, of a color and style complimentary to the building design.
- Ceramic tile: on decorative elements such as domes or parapets.
- High grade composition shake.

Rain gutters and downspouts should be concealed unless designed as a specific architectural feature of the building. Where rain gutters are exposed as a design detail, they should be of copper, or properly primed and painted metal of a color complimentary to the building design.

Flat Roofs - flat roofs should be detailed and of a material to provide a clean, uniform appearance from public areas and windows of adjacent buildings and should also be designed to screen rooftop equipment.

Awnings

Awnings provide the opportunity for color and visual relief. However, because of their essentially temporary nature, awnings should not be the dominant design feature.

- Awnings should be intermittent, limited to the width of the protected opening
- Awnings serve a very functional purpose by protecting shop windows from intense direct sunlight. The most functional awnings are "retractable", having a frame and support structure with the ability to be adjusted up or down depending upon lighting conditions.
- Awnings should be of a durable, commercial grade fabric, canvas or similar material having a matte finish. Awning frames and supports should be of painted or coated metal or other noncorroding material.
- Brackets and supports should be decorative, preferably of wrought iron, and designed to visually compliment the



Dormers provide visual relief along a long roof line and can accentuate important facade elements such as entrances.



Even in a retracted position, an awning can contribute positively to storefront design.

architecture.

- Glossy or shiny plastic or similar awning material is not recommended.

Miscellaneous Elements

Other ornamental design elements, many of them available prefabricated, can be used. These should be used judiciously:

Decorative metalwork

Metal Grilles are good ways to enhance a required vent or small opening. Flagpoles can enhance the civic quality of a facade.

Finials

These elements are a useful punctuation for a centralized element to accent the vertical.

Balconies

Whether useable or not, balconies lend the feeling of 'life' to the street. These should be substantial in design and use details found elsewhere in the facade design.

Side and Rear Building Facades

Building design character should be consistent over the entire building, especially where the building will be visible from publicly accessible areas, such as streets, alleys or parking areas.

Although details may be simplified, elements such as finish materials, cornices and window trim should be consistent on each elevation.

Blank Wall Areas

Walls without openings should be strongly discouraged at the street front. Where a building abuts a side property line at the interior of a block, and where that building's side wall is likely to remain visible for an extended period of time, that building elevation should reflect a design treatment consistent with the building's established street-front design. Corner buildings are considered to have two "street-front" facades. Thus windows on the side are strongly encouraged to be interesting and inviting.



An alley need not be an uncomfortable space. This alley had visibility from a major street, so designers seized the opportunity to provide an good quality facade.

Exterior Color Design

Exterior colors play an important role in the way we perceive a building and its details. Color can be used to draw attention to specific parts of a building, such as entries. Color can also be used to mask or diminish the visual importance of a particular feature, for instance a service area, simply by altering colors, or the contrast between surfaces and details, in a certain area.

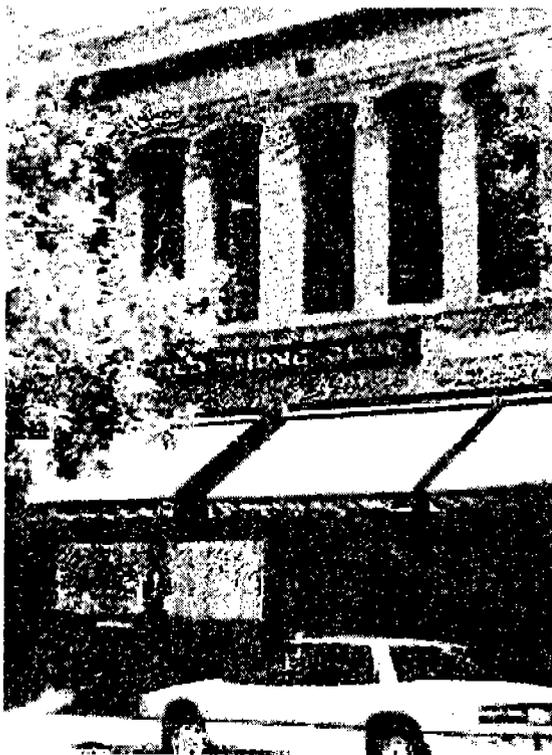
In general, contrast (light against dark or dark against light) will call attention, for better or worse. These issues should be considered as noted below:

Exterior Building Colors

These should be muted, while trim and detail colors should provide a contrasting accent, although exceptions may be made where justified by specific design intent, particularly if drawing on the area's rich cultural diversity or artistic background. Colors should also represent the architectural character of the building. Several paint manufacturers provide historic color palettes as a reference for period color design. Bright or intense colors should be used sparingly, and should typically be reserved for more refined or delicate detailing, such as grillework, as well as more transient features such as awnings, signs and banners. Avoid trendy or garish colors that may be incompatible with the consistency of the architectural character of the area. Neutral or light colored walls should be contrasted with a darker more intense trim color, while dark colored walls should be contrasted with light colored accents and details.

Finish Materials with Natural Colors

Materials such as brick, stone, copper, etc. should be used where practical and left in their natural colors.



Contrasting accent colors, such as the dark arched lintels above the second story windows, the dark strip in the cornice and the signage, bring out the subtleties of the architecture.

Additions, Renovations and Restorations

Many buildings within the Pedestrian Core district already possess the basic positive characteristics expressed in these guidelines, and care must be taken to not simply "change something for the sake of change."

In some buildings it may be desirable to simplify the front elevation, reducing the number of exterior materials, modifying a complex roof line, or removing a contemporary element which is hiding some original and desired character or detail. Modifications to buildings should be done with consideration for what is existing and how new work will relate to remaining materials and structure, especially in buildings having historical significance, where extreme care and respect for the historic character should be used to guide design.

Additions to Existing Buildings

Additions should be done with care and sensitivity to assure a compatibility between the old and new. In general, any addition to an existing building should reflect one of the following two basic approaches, unless the entire building character is being reinvented by the project:

Replication of Existing Character

The design incorporates the proportions, details and features of the existing building or adjacent buildings in a manner very similar or identical to the original construction.

Interpretation of Existing Character

The new design responds to the existing building "sympathetically." For example, using like proportions in combination with stylized or simplified details inspired by the originals.

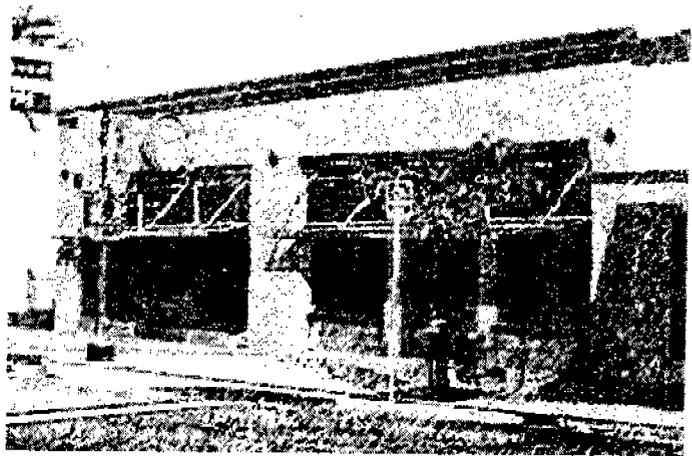
Seismic Retrofitting

Where structural improvements for seismic retrofitting are to affect the building exterior, such improvements should be done with care and consideration for the impact on appearance of the building. Where possible, such work should be concealed; where not possible or practical, the improvements should be planned to carefully integrate into the existing building design.

Seismic tie straps, to secure floor and roof framing to the adjacent walls, are one of the most common improvements made today. However, the straps are often installed with no guidance about the careful placement of the ties. Often the tie holes are drilled without alignment or through key features or details, resulting in a haphazard and destructive result which can permanently scar the building.



Before



After

Renovations of blighted buildings can have a dramatic impact on a neighborhood without an extravagant expenditure of funds. Effective use of paint, accenting existing architectural features, adding signage, awnings and cornices can have striking results. In this example, a boarded-up clerestory is revealed and enhanced.

Architectural Lighting

Lighting should be considered an integral part of building design. Proper lighting design will have a positive effect on the building and its users, as well as help to promote a safe and enjoyable nighttime pedestrian environment in the downtown core.

Types of Lighting

Within the Westside there will be a need for several exterior lighting types:

Area Lighting

This is for public and private parking lots, alleyways, parkways and walks. Such lighting should be set in a manner that assures maximum lighting benefit without allowing stray light to intrude into windows of nearby residences or to create glare problems for nearby automobile traffic.

Ornamental Lighting

This is not intended to light large areas or wall surfaces. Ornamental lighting plays an important role in bringing visual life to streets at night. Colors, finishes and design of ornamental lighting should be compatible with the design characteristics of the architecture.

'Hidden Source' Lighting

For certain prominent architectural features, hidden source lighting can be used to create dramatic effects, illuminating towers or other unique architectural characteristics. Such lighting can be concealed in soffits, behind ledges or parapets, or even set into landscape areas with the light directed at the desired element to be highlighted.

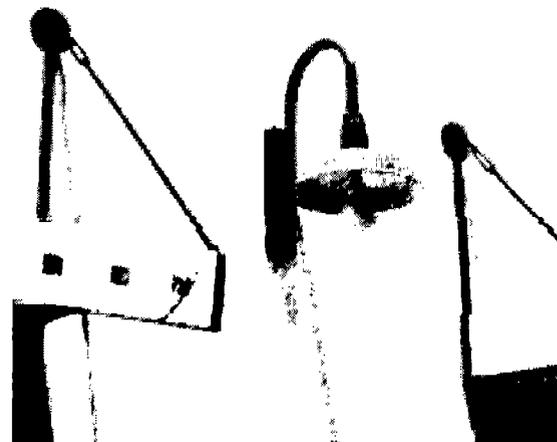
Design Integration

It is recommended that a lighting design professional be consulted as an integral part of the design process for determining types and intensities of lighting to be used. In addition, the following "rules of thumb" should apply:

- Use the minimum brightness necessary for illumination of large areas.
- Use brighter, more intense lighting to highlight architectural



Effective lighting can enhance the architecture and provide drama for the evening pedestrian.



Simple lighting can be decorative and relate to other elements. In this case, the metal awning support rods are similar to the gooseneck lamp support.

features and focal points.

Light Fixture Materials

Lighting fixture materials should be compatible with the design objectives of this Plan. Where lighting is mounted to a building, its materials, finishes and colors should complement and coordinate with those of the building.

Recommended Lighting Glass and Globes include:

- clear acrylic or polycarbonate with fresnal patterns;
- borosilicate glass;
- translucent clear-frosted or white acrylic or polycarbonate globes.

Glass and Globes which are **not** recommended:

- clear or tinted smooth finish acrylic or polycarbonate globes;
- any dark-tinted or mirrored glass diffusers

Recommended lamp types include:

- color corrected high pressure sodium (HPS);
- color corrected metal halide (3,000? K);
- color corrected standard fluorescent (2,700 -3,000? K).

Lamps which are *not recommended* include:

- **Standard** "peach" high pressure sodium;
- low pressure sodium;
- standard mercury vapor;
- non-color-corrected, standard fluorescent.

Sign Design

Commercial signs are one of the most important elements of a healthy business district. Signage communicates more than just the name of a business. It communicates the degree to which the businesses in that core are alive and whether or not those businesses still care about the business they are doing.

A well-maintained sign, with bright colors and lighting that is functional, says to potential customers "We are here, we care and we want your business." Likewise, a dull and worn sign says "We are tired and may not be around much longer - your business isn't that important to us."

In addition, signs at the pedestrian level are important and can add significantly to the pedestrian experience when done well. The sign design guidelines included herein are intended to encourage high quality sign design. Such signs should be considered as "calling cards," expressive of the spirit and enthusiasm of the business within. Signs should be positioned in close proximity to the business entry wherever possible.

These guidelines are intended to encourage excellence in signage, as a communication tool and as an art form, to attract customers, make a statement about the quality of the businesses represented, and to contribute positively to the overall quality of the Pedestrian Core. Businesses are encouraged to use signage in a manner that will clearly express the spirit of their unique enterprises while at the same time enriching the visual life of the street for everyone.

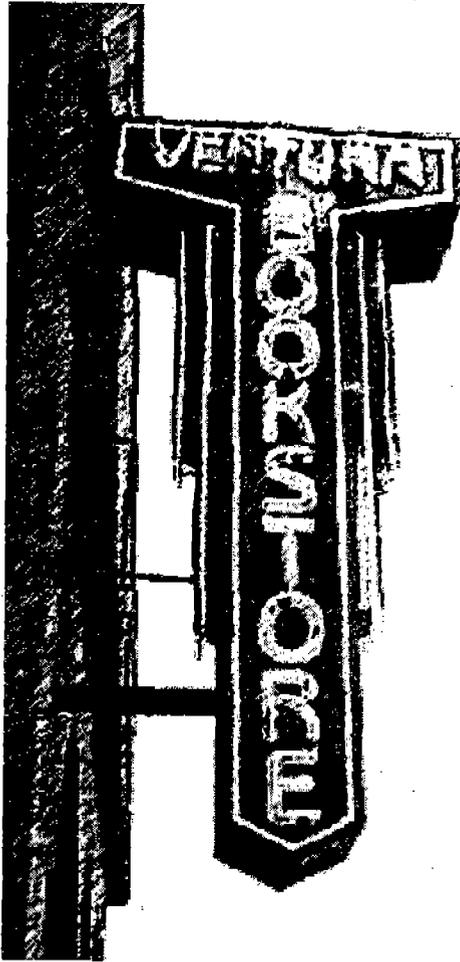
Types of Recommended Signs

The following sign types are recommended within the Pedestrian Core:

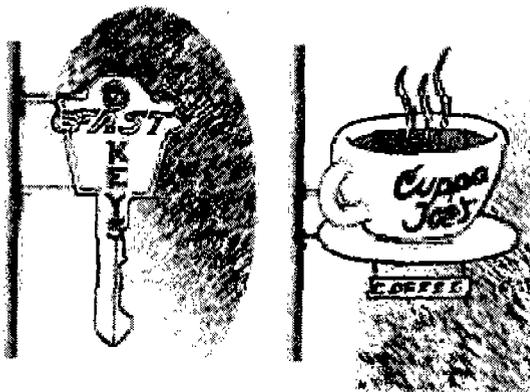
Wall Mounted Signs

Wall-mounted signs are attached panel or individually mounted letters. Wall signs should be located on the building face in an area specifically intended for signage, or where such signage will appear integral to the building design.





A high quality sign is often perceived to be an indicator of the quality of the business.



Figurative signs are not only effective for the pedestrian, but for passing motorists as well.

Painted Wall Signs

Graphic art, applied directly onto the building exterior wall surface in an appropriate location.

- Such signs should not block or cover any significant architectural features or details of the building.
- Wall mounted "can" or cabinet type wall signs are *not encouraged*.

Projecting Signs

Projecting signs may come in two types: as a main wall sign or as a pedestrian oriented sign placed under an awning or canopy. Projecting pedestrian signs are limited to a six square-foot maximum. Projecting signs may be attached to the building such that no portion of the sign extends lower than seven feet six inches (7'-6") above the adjacent finished grade, nor should any portion extend beyond about three (3) feet from the face of the building wall.

Projecting signs may be directly attached to the building face, or suspended from decorative brackets.

The sign face and structural support should be designed as a compatible unit and should be attached to the building in a manner that will not damage or interfere with the building character.

Figurative Signs

These are signs that advertise the occupant business through the use of graphic or crafted symbols, such as shoes, keys, glasses, books, etc. These types of signs are encouraged where appropriate. Figurative signs may be incorporated into any of the allowable sign types identified here.

All sign attachment accessories on the same building should be coordinated.

Applied Window Signs

Window signs must be applied or painted directly onto the glass, and should not exceed twenty five (25) percent of the window area at street level. Window signs should not be painted over an opaque background, in order to preserve transparency of the window.

Directional, Alley and Service Signs

These signs should be discreet and integrated into the design of the building being served.

Murals

Murals should be subject to review and approval.

Awning and Canopy Signs

Such signs should be integrated into the design for the awning or canopy, subject to the following:

- Sign area should be limited to fifty percent of the total surface area on which the sign is located (typically the valence).

Sign Design Integration

In all cases, the location, style and detail of the sign should be compatible with, and reflect the spirit and quality of, the Pedestrian Core, utilizing text, graphics and design features that promote the area as a high quality, pedestrian-friendly district.

Allowable Sign Area

Refer to City Sign Ordinance for sign ratio. Signs should be in proportion to the building's scale and design. Sign variances will only be granted where it can be demonstrated that the proposed design is beneficial to the overall quality of the building design and district character.

Sign Illumination - Lighting should be considered as an integral part of sign design, and may be either internal or external, with the following considerations:

Where signs are externally illuminated and light fixtures are visible, they should be compatible in character with both the sign and the building design.

Outside lighting should be directional, and should be positioned such that no glare or hazard is created for onlookers.

Recommended Materials

The following materials are recommended for use in signs:

Wood - carved, sandblasted, etched, and properly sealed,



A variety of signs give pedestrians visual interest as well as information.



Light Fixtures come in a wide variety of shapes, styles and colors. Playful signage can be enhanced by using complementary lighting.

primed and painted, or stained.

Metal - formed, etched, cast, engraved, and properly primed and painted or factory coated to protect against corrosion.

High density preformed foam or similar material - new materials may be very appropriate if properly designed in a manner consistent with these guidelines, and painted or otherwise finished to compliment the architecture.

Unique materials and designs which are not identified here, but which are compatible with the objectives of these Design Guidelines may be considered, subject to review and approval.

CONNECTING AREAS

Intent of the Designation

The areas along Ventura Avenue between the Pedestrian Core Areas (PCAs) are designated as within the 'Connecting Areas.' These areas are similar to the PCAs in the following manner:

Pedestrian comfort and safety will be ensured.

Building frontages should orient to the street.

The existence of light industrial and auto-related uses along Ventura Avenue is a reminder of the rich commercial past of the Westside. These uses are not considered undesirable within the district.

Character of the District

The Connecting Area is intended to contain the variety of uses occurring there now. This includes commercial retail, auto-related commercial, institutional (such as schools and churches) and residential. These guidelines seek to allow for this mix while defining improvements that will result in a cohesive unity within the district. While the public improvements, including street trees, paving, etc., will go a long way to give this visual continuity, the private improvements must also respond to enhance the experience of both the pedestrian and motorist.

In the PCA, it is strongly recommended that buildings be built right to the property line. Since the PCA stresses commercial retail, this close proximity to the street is generally desirable. However, because of the variety of allowable uses within the Connecting Area, building to the right-of-way does not necessarily jibe with the existing or intended use of the property.

For instance, ground floor residential development should have some buffer from the activity of the street. The best response is to set residential units back (about 10 feet minimum) and up slightly (about 4 feet), with entries and windows maintaining strong connection to the street. Likewise, auto-related commercial uses must balance good visibility with the need to buffer (protect) passersby from noise and

fumes inherent in conducting their business. These guidelines attempt to reflect these differing needs but also encourage project design to provide the elements necessary to enhance the street experience within the Area.

Commercial Retail should follow the guidelines set forth by the PCA.

Commercial

The Connecting Areas, in order to emphasize pedestrian-friendly street frontage, can have exclusively commercial retail uses at the ground floor level at the street. Multistory buildings can have more commercial retail on the upper levels or have a mixed-use occupancy. Commercial Retail should follow the guidelines set forth by the PCA.

Mixed Use

Commercial/ Residential

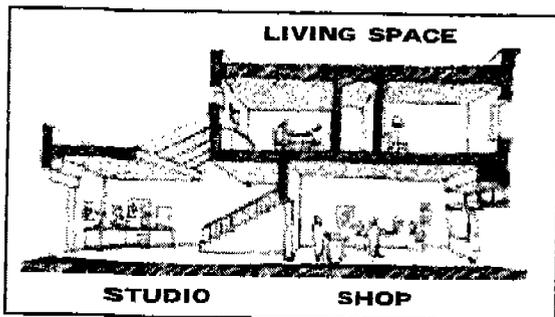
Residential units above commercial were a common configuration in older American cities where land was a premium and the ownership of automobiles was not commonplace. Advantages of residences within commercial areas include proximity to shopping.

Commercial/ Office

Neighborhood-serving businesses, such as travel and insurance agents, doctors, attorneys, etc. and many small businesses find that office space in these urban areas can be conveniently reached as well as having associated businesses (copy shops, banks, restaurants) nearby.

Live/ Work, Artist Lofts

Live/Work is a relatively new term for an old concept in which a business proprietor resides above his or her shop. This is often a desirable situation for security and longer hours of operation. There is a thriving art community in the Westside, so establishing Artist's Lofts in which an artist resides, conducts their particular trade and may sell their wares, is a unique possibility.



Artists Lofts or other Live/Work configurations can provide activity day and night.

Light Manufacturing/ R&D Facilities

Industry has formed the backbone of the rapid expansion in the early part of this century. However, the oil-related heavy industries have, for the most part, left the area. Light industry appears to be the most prevalent form of industrial use- small

businesses housed in relatively small, simple shop spaces.

Multi-unit Housing

Successful communities have a diversity of housing types, including rental and owner properties. Multi-unit housing is an appropriate intensive use in the Connecting Areas, similar to upper living housing in the PCA. Proximity to services and transportation can make this housing choice attractive, while allowing a human presence during non-business hours. Unlike the Core Area, housing can be placed at the ground floor level.

Edge Condition

The emphasis regarding the interface between the street and private property is primarily concerned with the edge condition- how does the private development meet the public right-of-way.

Setbacks

Commercial retail should, as in the PCA, be built to the property line for direct access from the sidewalk. Parking should be configured to the rear of the project to the greatest extent possible.

Light industrial facilities and auto-related commercial and other uses that require a high degree of open lot space should place as much of the building as possible along the street frontage, with minimal view from the right-of-way of the work bays or outdoor storage areas, etc. Further containment can be achieved using fences.

Residential projects should be setback a minimum of ten (10) feet. This allows an appropriate buffer from street and sidewalk noise and also open porches and garden space. The use of a fence can also accentuate this separation.



Setbacks at residential projects can be landscaped and support front stoops or porches for residents.

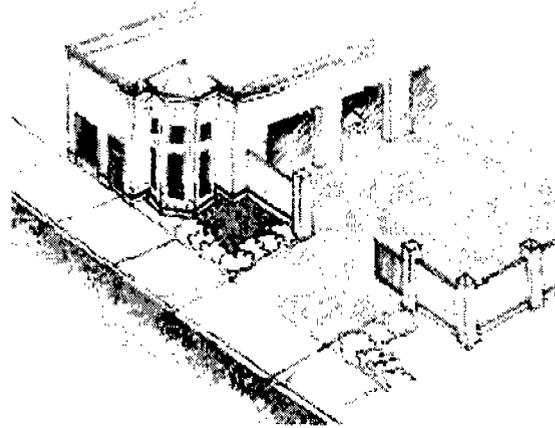
Fences

Although most thought of as devices for security and screening, the design of fences is important because they contribute to the continuity of the street edge, helping to define the public right-of-way and provide visual interest for pedestrians and motorists.

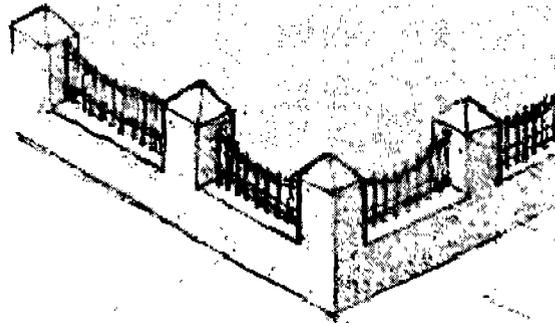
As in the design of building fronts, fences should be of good quality and appropriate materials. If a fence is placed in conjunction with a structure on the street edge, it should complement the building, using like materials, rhythm or details. Fences should not appear monolithic, such as a continuous unbroken block wall or insubstantial like a chain-link fence. Fences can also be used as a surface for effective signage.

Residential

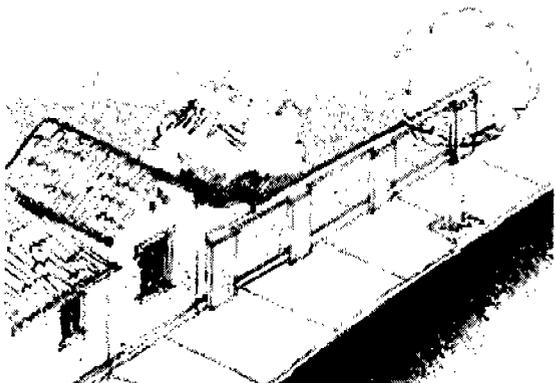
The separation, and thereby the privacy and buffer, afforded by the setback can be further enhanced by the placement of a fence at the property line. Tall fences should have a degree of transparency, such as wrought iron, while shorter fences can be solid, such as stone or brick. This adjacency to the right-of-way calls for durable materials and a design quality that relates to the strolling pedestrian.



Quality site design, such as for this auto repair facility, can benefit community design (in this case minimizing views of the work bays, providing landscaping) as well as the building owner or tenant (having the sales office at the street, a secure yard area.)



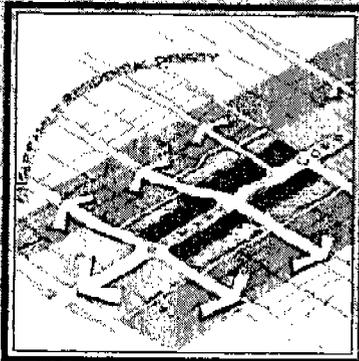
Fence design should be given the same consideration, such as mass and rhythm, as building design.



Fence design should be integrated into the building style where possible.



Implementation



IMPLEMENTATION

As a refinement of the Westside Community Consensus Plan, this document identifies many important objectives and improvements that can occur throughout the project area. Some of these are doable *today*, some in the immediate future, and some far-reaching and in need of further study. Because these improvements include both public and private projects, the responsibility for success must be shared.

Providing for incremental change ("little victories") is just as vital as preparing a long-term plan. When residents, property owners and businesses are shown the hope of a new vision, and then agree to support it, incremental degrees of success become essential in maintaining their support. Even the smallest improvements – a new sign or freshly painted storefront – can be a symbol of success, making the difference between generating positive momentum.

Most citizens want the community's plans to succeed, but they need tangible results to hang their hopes on. With this in mind, an implementation strategy must be developed, which helps the project stakeholders see early success, both public and private, while working to attain the longer-range vision.

PUBLIC IMPROVEMENTS

There are a number of recommended public improvements identified within this plan, including improvements within the street right-of-way, as well as some suggested improvements to public facilities, such as the Adult Center at Simpson Street and Ventura Avenue. In addition, improvements of a "public nature" are recommended on private property which is currently under quasi-public use - specifically at the mini park adjacent to the Adult Center.

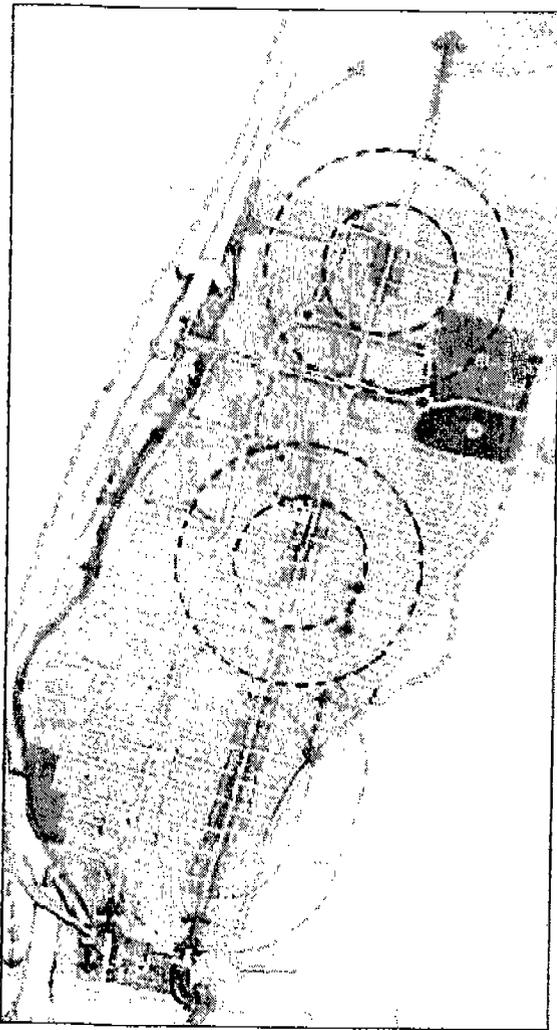


Most citizens want the community's plans to succeed, but they need tangible results to hang their hopes on."



The Westside Consensus Plan, 1997.

With this in mind, an implementation strategy must be developed which helps the project stakeholders see early success, both public and private, while working to attain the longer-range vision."



Recommended Strategic Plan for Westside, 1998.

Identifying Costs and Setting Priorities

Understanding that the community's resources are limited, the first step in implementing any significant plan for change is to identify costs and priorities of the key components. The proceeding chapters outline several areas of improvement within the public realm which the community has identified as being desirable, needed or beneficial to the community.

Some of these improvements pertain to the appearance and comfort of the existing business districts within the Westside. Others address functional and safety concerns, especially with regard to traffic circulation and safety for both vehicles and pedestrians. A third category pertains to public facilities, such as parks, which the community feels are not presently provided on an adequate level.

These improvements are extensive and diverse and represent significant public investment in the area. The following cost estimates are not intended to be definitive, but are provided as a basis for comparison and prioritizing. As of this date, the community has not established an order of priority for implementing these improvements. Further, several of these projects will require much more detailed planning and design, as well as property acquisition and, in certain cases, remedial site preparation, all of which may affect the priority setting process.

This being said, careful consideration should be given to emphasizing those improvements which will likely have a direct and immediate beneficial impact on public and business perception about the area. The success of this revitalization effort hinges upon the degree to which public investment catalyzes private response. It is the private response, ideally representing significant increase in business and investment activity within the planning area, which will result in direct return on the public's investment, manifest in increased property values and property and business tax revenues. Therefore, those improvements which offer the most immediate impact on community perception should be carefully considered as projects of highest priority.

Public Improvement Project Cost Estimates

The following cost estimates are not intended to be definitive, but are provided as a basis for comparison and prioritizing.

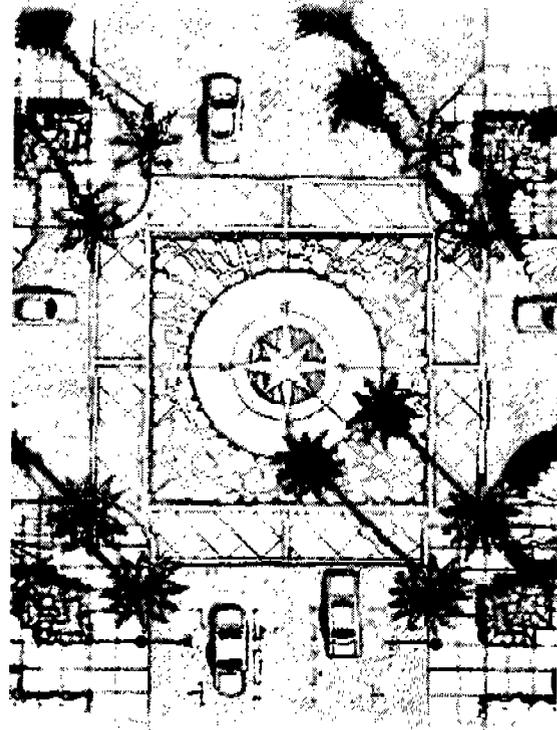
Public Right-of-Way Projects:	Anticipated Costs:
1. Street/pedestrian Improvements: includes enhanced sidewalk paving, intersections, street lighting, urban furnishing, street trees and traffic calming improvements.	
a. Ventura Avenue	
i. Primary core area (5 blocks) estimated cost per block - \$250,000 cost for five blocks -	\$1,160,000
ii. Secondary core areas (2 x 2 blocks ea.) estimated cost per block - \$250,000 cost for each secondary core -	\$ 500,000
total for both secondary core areas -	\$ 925,000
2. Public Plaza/alley adjacent to Adult Center for improvements, furnishings, utilities, lighting, reconfiguration of alleyway (does not include acquisitions)	\$ 265,000
3. Gateway improvements	
a. South ends of Ventura Ave. and Olive Street 100,000	\$
b. Intersection of Ventura Ave. and Stanley	\$ 60,000
c. Stanley Avenue at Hwy. 33	\$ 40,000
4. Street tree planting program All streets w/o existing street trees (estimated 1,200 trees @ \$300 ea.)	\$ 360,000
5. Cedar Street Bike/Ped. Path (2,400 lf @ \$200/lf) 480,000	\$
	\$3,390,000
6. Westside Regional Park	<i>costs to be determined</i>

Additional Street Improvement Projects

1. Olive Street Extension	\$2,500,000
2. Cameron Street through-connections	\$2,000,000
	\$4,500,000

Total Anticipated Public Improvements Expenditure \$7,890,000

As previously stated, these estimates are provided as a basis for comparison and initial project planning. More thorough and detailed plans and estimates for each project will be needed as the revitalization process unfolds.





Maintenance

Public improvements will require on-going maintenance at various levels, depending upon the particular type of improvements in question. A formula for estimating on-going maintenance of street improvements of the type envisioned here consists of a square foot cost per year in the range of .35 to .45 cents. At .40/sf a 10 foot wide 300 foot long improved sidewalk with modest planting areas, street trees and lighting would cost approximately \$1,200 per year to maintain. Annual maintenance costs for improvements along Ventura Avenue could thus be estimated as follows:

Primary core area (approx. 1,850 lf x 2 sides):
 $3,700 \text{ lf} \times 10\text{ft width} \times .40/\text{sf} = \$14,800 \text{ per year}$

Secondary core areas (approx. 400 lf x 2 sides x 2 areas)
 $1,600 \text{ lf} \times 10\text{ft width} \times .40/\text{sf} = \$6,400 \text{ per year.}$

Similarly, maintenance of the proposed plaza can be estimated as follows:

Plaza area:
 $5,625 \text{ sf} \times .40/\text{sf} = \$2,250 \text{ per year.}$

Using a similar formula, adjusted by 12% for economy of scale, maintenance of a public park facility of approximately 20 acres would be as follows:

Regional Park:
 $871,200 \text{ sf} \times .30/\text{sf} = \$261,360 \text{ per year}$

Note that these figures are very general, not design specific, and do not address any increase in costs which may be due to a need for additional staffing program specific requirements.

Public Policy and Planning

Additional implementation costs will likely be incurred in the form of staff and consultant time to prepare and process additional planning documentation, as well as any necessary amendments to existing documents, including the City's General Plan and Zoning Ordinance. Potential work and associated estimates of cost, (including staff costs) identified thus far includes:

1. Westside Urban Design Plan Adoption

In order to begin the process of implementing this Plan, the City must formally adopt it in conjunction with a funding mechanism, most likely in the form of a resolution by City Council, with instructions to

staff to take appropriate next steps.

Anticipated Cost: Included in present effort.

2. Plan and Policy Consistency

The Westside Urban Design Plan identifies various development - related opportunities that may or may not be consistent with current City Plans and Policies. The Comprehensive Plan, Zoning Ordinances and related documents may require updating and/or revision if significant differences exist between these and the vision of this document. As an option to amending the City's existing zoning ordinance, overlay zones may be created to address specific area objectives within the Westside.

Anticipated Cost: \$70,000 to \$180,000

3. Redevelopment Area Potential

Certainly the potential for a significant redevelopment effort is present in the Westside Community. Redevelopment brings with it several mechanisms for encouraging both public and private investment, and a powerful financial mechanism for accomplishing public improvements.

Anticipated Cost: \$120,000

4. Environmental Impact Studies

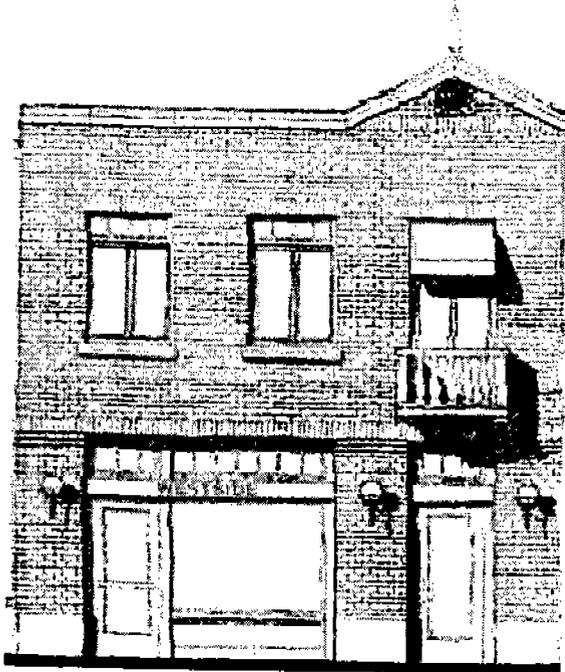
CEQA, the California Environmental Quality Act, requires that projects which represent significant change must be assessed with regard to potential impacts on existing environmental conditions. Although such impacts may be considered "positive" or "negative," an EIR is intended to measure the degree of change anticipated and the potential impacts such changes will have on the area in question. The City may choose have a consultant prepare a "master EIR" to address the broader issues and impacts of the total plan, while requiring "focused EIRs where specific project impacts are at issue.

Anticipated Cost: \$170,000

5. Private project incentive programs:

The City (or potentially a redevelopment agency) may seek to establish some programs to provide financial incentive to the private sector to facilitate initial investments in private property within the plan area. Such programs include, but are not limited to:

- a. Facade renovation incentives \$200,000
Such a program would require seed capitol to be made available to property and business owners in the form of small grants or low-interest loans to renovate building facades.
- b. New in-fill development incentives \$200,000



Such a program would require seed capital to be made available to property and business owners in the form of small grants or low-interest loans to develop under-utilized property in strategic locations.

PRIVATE IMPROVEMENTS

There are a number of opportunities for new in-fill development within the Westside Community, including new commercial and mixed-use projects which would be supportive of the goals of the community.

The Plan also identifies the significant opportunity in the properties surrounding Stanley Avenue to establish a more cohesive business district to supplement the loss of oil-related uses in the area. Stanley Avenue enjoys easy access to the freeway and is anchored by a significant corporate user in Kinko's. The suggested strategy acknowledges this facility as a tremendous asset, and seeks to encourage more and similar uses in the proximity.

The community participants were clear in their concern for the value of existing businesses and their desire not to destroy the historic and very human scale of the Westside Community in the quest for new community investment. The desire is to support development which contributes to the community's existing positive qualities of scale and access.

FUNDING

Capitol Improvements

As indicated above, this document identifies numerous capitol improvements to be made in the public right-of-way for which there is currently little or no funding set aside in the City's Capitol Improvements budget. Furthermore, there are long-term maintenance implications associated with specific capitol projects and additional planning studies required to implement various facets of the plan. Potential funding sources include:

- CBDG (Community Development Block Grant) Funds;
- Categorical State and Federal Grants
- Redevelopment Tax Increments;
- Benefit Assessment Districts;
- Capitol Improvement funds;
- City General Funds.

As previously indicated, the cost estimates provided herein are not intended to be definitive, but are provided as a basis for comparison and prioritizing. More specifically, this Plan is not intended as a budget document nor by its endorsement would the City be committed to fund any or all of the plans, programs, or projects proposed.

Rather, the Plan provides the basis for setting priorities and evaluating funding as part of the ongoing budgetary and policy-planning process.

PLAN ADOPTION

The Westside Urban Design Plan is meant to serve as a community tool for change and investment by both the Public and Private sectors. Although this document itself does not hold the power of ordinance, it is recommended that this document and the concepts and design recommendations within will become the basis for additional action on a number of levels, including:

- Review and endorsement of the overall Plan by the Westside Community Council, Design Review Committee and Planning Commission;
- Adoption of the Private Realm Development Guidelines as a component of the City's Design Review Guidelines.
- Review of the overall Plan by the City Council with specific action on implementation as deemed appropriate.

The Plan is presented to the City Council as an advisory document in the same context as the Council previously considered and accepted the Westside Consensus Plan in 1997. Accordingly, the Council's acceptance of the Urban Design Plan does not have a legally binding effect on later activities. Rather, the Plan provides the basis for actions to follow.

Finally, the Urban Design Plan represents a further refinement of the concepts set forth in the Westside Consensus Plan. In essence, the Urban Design Plan is part of an ongoing and evolutionary process that is founded on earlier community-based planning efforts. In this same context, it is recommended that the planning process should continue, in an effort to refine and formalize the planning strategies described herein. Most importantly, the Plan's success depends upon and presupposes a collaborative City and community-based implementation process, in which the citizens of the Westside continue to take an active leadership role.

